

Te Pūmanawa o Tamaki – DTC Redevelopment.

Precinct Properties New Zealand Ltd.

Landscape Assessment.

December 2024

Isthmus.



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Note, this report has been updated in response to section 92 requests and changes to the design as a result of refinement and development.

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INTRODUCTION

Background

- 1 This application is for a proposed mixed use building development by Precinct Properties New Zealand Ltd at 2 Lower Hobson Street, Tāmaki Makaurau Auckland (**'the site'**). The proposal provides an integrated design approach with a number of built elements, which includes two towers, a podium and public realm design, which will enable transformative outcomes to downtown Auckland.
- 2 Isthmus Group Ltd (**'IGL'**) has been engaged by Precinct Properties New Zealand Ltd (via the project management team at RCP) to undertake a landscape assessment of the proposal. The purpose of the report is to support a resource consent application to Auckland Council.
- 3 The key matters discussed within this report relate to the quality and amenity values of the proposal's built form and character in this central city urban landscape. This includes the design of the buildings and public realm (landscape) components, and assessment on any potential visual amenity effects of the proposal in relation to the localised streetscape and wider environment, including positive effects. It also assesses the design's appropriateness in relation to the site's location proximate to the Auckland waterfront and harbour edge.
- 4 The project has been designed by Warren and Mahoney Ltd and Snøhetta's architecture and landscape architecture teams, as part of a wider project team which includes McIndoe Urban and other technical specialists.
- 5 The proposal has been subject to review by Eke Panuku's Technical Advisory Group (**'TAG'**) on four separate occasions, working through the refinement of the design and providing general support, overall¹. Formal reviews were held on 14 April 2023 (TAG 01), 19 May 2023 (TAG 02), 28 July 2023 (TAG 03) and 10 May 2024 (TAG 04). Informal design workshops were also held on 26 January and 15 March 2024. The minutes of each meeting are appended to the Assessment of Environmental Effects (**AEE**) prepared by Barker & Associates Ltd.
- 6 This report is supported by a graphic attachments document which includes a series of panoramic photographs and visual simulations which provide site context (refer **Appendix B** to this report). A series of design documents

¹ Within the TAG Review (04) Recommendations Note (dated 10th May 2024), the members stated that they consider "...that the design process had reached a point where all of the major urban design issues have been convincingly addressed by the consultants, and that the design proposals now demonstrate a high level of urban design quality and resolution."

prepared by Warren and Mahoney (**WAM**) provide the relevant design drawings and rationale for the proposal. These documents are referred to as necessary throughout this report² and are included as part of this resource consent application.

- 7 Also included as part of this resource consent application is a separate **Urban Design Assessment** which has been prepared by McIndoe Urban. That assessment and this landscape assessment report are complementary and address similar issues when assessing the proposal. This is outlined further below.

Report Update

- 8 This is an updated version of the landscape assessment report from that which was included as part of the original resource consent application. This is in response to the (i) Section 92 requests for further information received from Auckland Council (dated 23 September 2024), and (ii) a process of design development post application. Detail around these changes is outlined further within the planning statement by Barker & Associates Ltd, and summarised within paragraph 23 below. This has also resulted in updates to the Visual Simulations, which are appended to this report as **Appendix B**.
- 9 Although additional commentary is provided in response to the Section 92 requests, the overall conclusions remain as per the original assessment report.

METHODOLOGY

- 10 A methodology statement is attached as **Appendix A**. The methodology is consistent with *'Te Tangi a te Manu: Aotearoa New Zealand Landscape Assessment Guidelines'*³.
- 11 In summary, the method entails:
- (a) An outline and description of aspects of the **proposal**,
 - (b) Review of the relevant⁴ **planning context** (including objectives and policies) to help frame the assessment,

² Prepared by Warren & Mahoney and Snøhetta, dated December 2024. These documents are *Pūmanawa Downtown West* (Resource Consent): (referenced within this report as *WAM Document x*, etc)

- Document A – Proposed Architectural Drawings,
- Document B – Proposed Landscape Drawings,
- Document C – Existing & Demolition Drawings,
- Document D – Architecture & Landscape Report,
- Document E – GFA & AFA Schedule & Plans, and
- Document F – Shading Analysis.

³ *'Te Tangi a te Manu: Aotearoa New Zealand Landscape Assessment Guidelines'*, Tuia Pito Ora New Zealand Institute of Landscape Architects, July 2022.

⁴ Related to landscape assessment matters.

- (c) Description of relevant attributes of the **existing environment**,
 - (d) Identification of **key matters** with regard to the proposal, provisions, and context, and
 - (e) Assessment of **effects** with respect to the key matters identified, including **visual amenity**.
- 12 The method for preparing the visual simulations and the panoramic context photos is included within the A3 graphic attachments document (**Appendix B**). That method is consistent with the *NZILA Best Practice Guide 10.2, Visual Simulations*.

THE PROPOSAL

- 13 The proposal is described in detail within the ‘project description’ section of the AEE prepared by Barker & Associates Ltd and depicted in the aforementioned *design documents*.
- 14 The proposal is for a comprehensive mixed-use development at 2 Lower Hobson Street in the ‘western CBD’, which will enable urban transformation in this part of Auckland city. The site has two primary street frontages, one to Lower Hobson Street to the west and the other fronting Customs Street West to the south, with a secondary frontage and connection to Lower Albert Street.
- 15 The proposal will demolish and replace the existing Downtown Carpark (**DTC**) building⁵ and includes public realm design and modification to the podia which ‘wrap around’ the adjacent existing AON House and HSBC buildings (refer **Image A** below).
- 16 The design includes two towers with commercial office and residential uses, set upon a podium which includes three separate buildings. These elements will define and activate the street edges. On the ground level is Te Urunga Hau (the Urban Room) which is a public space and includes a series of pedestrian laneways and gathering areas.
- 17 The design has been developed through collaboration with design partners Haumi and Ngāti Whātua Ōrākei.⁶ This process has enabled a cultural narrative to be established which reflects Te Ao Maori principles, the importance of the site, its context in Tāmaki Makaurau, Auckland and the connection between the land and sea. The following statement (iho) forms part of the vision for the project:

⁵ Together with the Lower Hobson Street pedestrian bridge and Customs Street West vehicle ramp located within part of the road reserve.

⁶ The landscape assessment was prepared following review and understanding of the cultural narrative of the design process, documentation and outcomes sought.

Whakairo i te wai

Carved by water

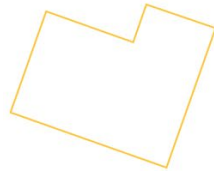
Whakairo i te ringa

Carved by hand

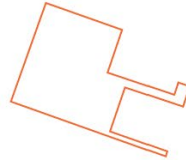
Whakairo i te rā, ā, ka ita i te taiao

Carved by light

18 This is outlined and discussed further within Section ‘1.4 Cultural Narrative & Hierarchy’ of the WAM Document D.



Scope of Land Use consent application comprising 3 contiguous sites - Downtown Carpark, Aon House, & HSBC Tower.



Approximate scope of physical works / demolition scope within the site boundary.



Additional demolition scope beyond the Land Use application boundary.



Image A: Site extent (not to scale).
(Sourced from the WAM Document D – Section 1.1).

19 The following aspects, in summary, are relevant to landscape assessment:

Building elements.

20 The design proposes two individual towers, namely T1 and T2 set upon the podium, and above six basement levels which includes car parking, bicycle

parking, loading docks, residential storage, generators, plant rooms and service areas. These buildings have different heights and are offset from each other (e.g. T1 is setback further than T2).

Towers.

(a) T1 – Tower 1

Tower T1 is a 56-storey mixed-use building which is RL227.2m in height (to the parapet). It is set upon the podium building P1 and has 41 floors of office space (levels 9 – 21, 23 – 50). Refer **Image B** below.

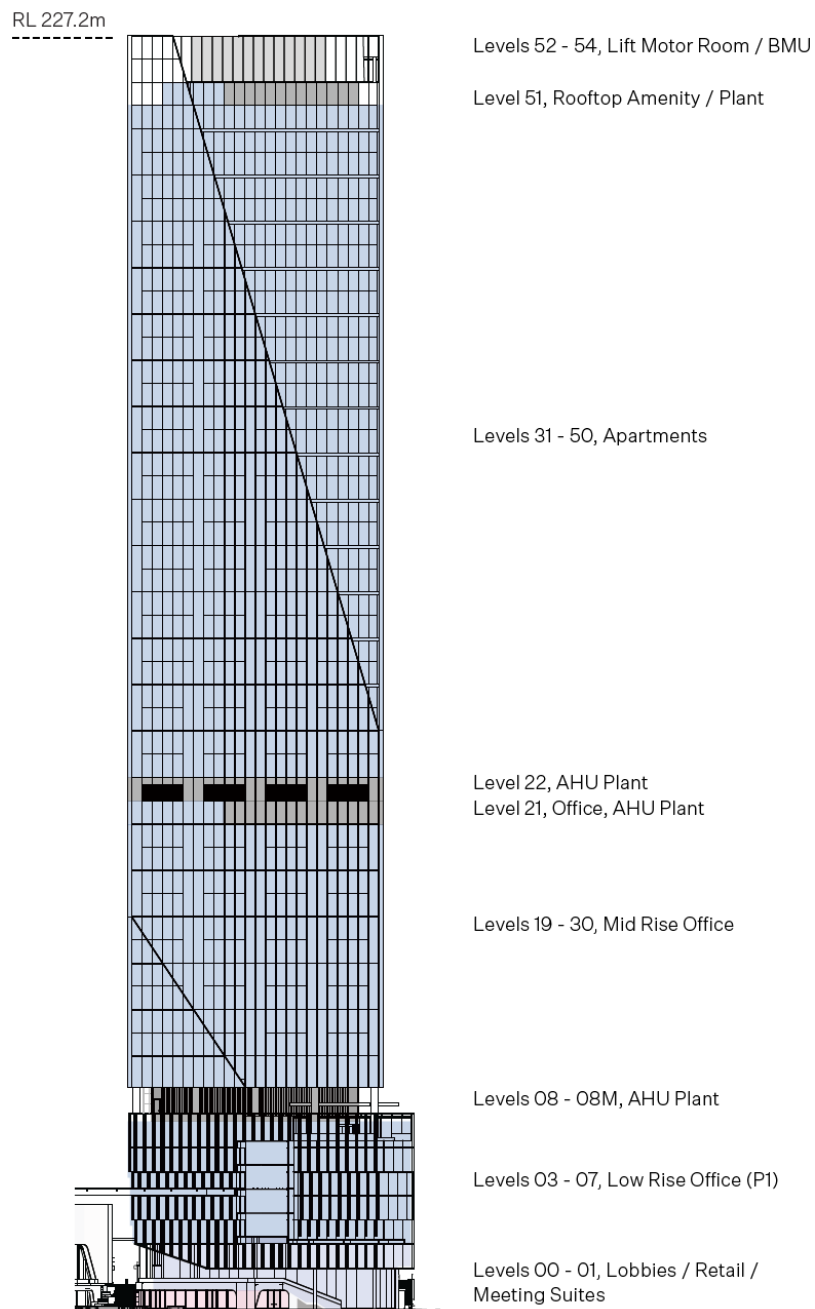


Image B: Tower 1 Elevation Diagram (not to scale).
(Sourced from the WAM Document D – Section 3.2).

(b) *T2 – Tower 2*

Tower T2 is a 45-storey building which is RL161.9m high (to the parapet). It is also set upon the podium (above building P2) and the design includes 34 floors of residential (levels 08 – 41) with some 247 apartments proposed. Low Rise Amenity⁷ is provided at level 7. Refer **Image C** below.

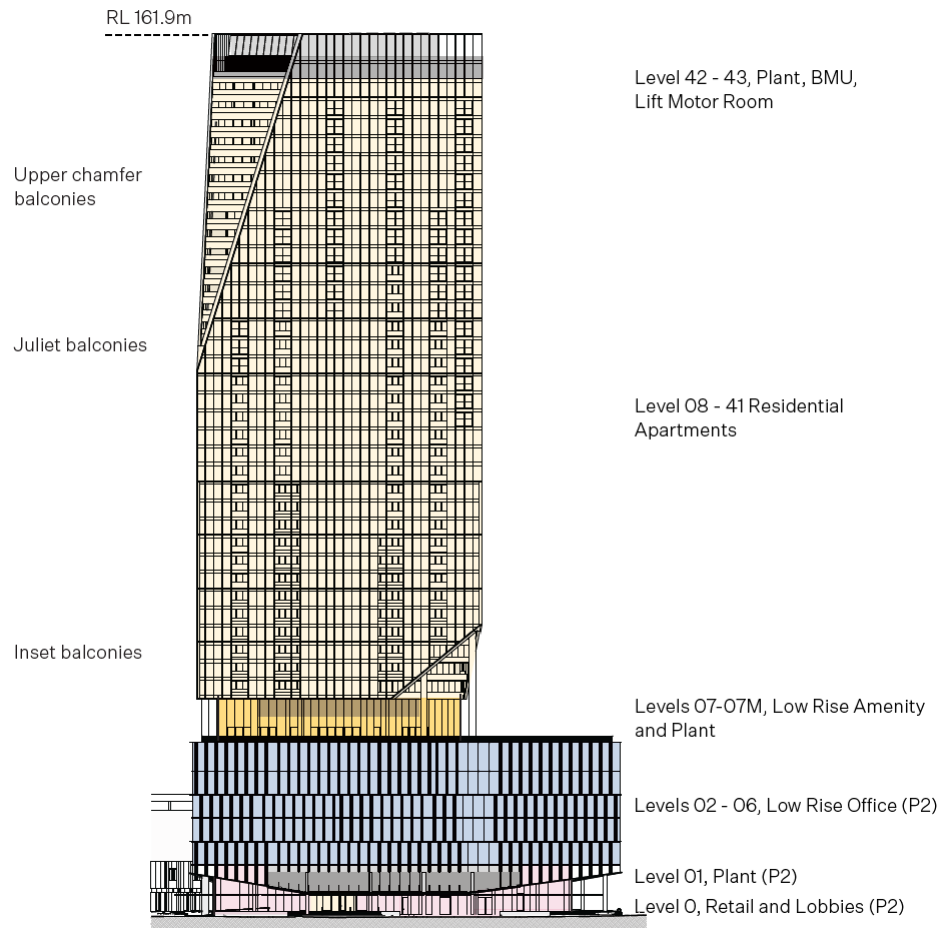


Image C: Tower 2 Elevation Diagram (not to scale).
(Sourced from the WAM Document D – Section 3.3).

(c) *Building Form*

The towers have the same architectural language with irregular shapes which have been designed as complementary, slender forms. The design strategy has been informed by the iho and have shaped the towers. This includes the distinctive chiselled / chamfered form of the upper and lower floors and corners. They are treated differently so that they are not identical but have a ‘family’ likeness. This chamfered form provides a connection and ‘carving’ toward the respective harbours to the northwest, northeast and south, and stepping back of these parts of the building as if carved by light in response to sunlight compliance matters.

⁷ The residential amenity levels include components such as a pool, spa, sauna, gym, dining, kitchen, lounge and roof terrace.

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It also includes vertical extension of the façades which will screen the rooftop plant elements, and the lighting of the upper 'carved' elements sought to provide contribution to the city skyline.

(d) *Materiality – Tower Façades*

Similar to the building form the materiality is designed with similar, yet not identical, façades. Both towers include a glazed unitised curtain wall system with vertical fins (which include fritted glass) which help subtly express the verticality of the building, provide depth and shadow, and provide a differentiation between the top and bottom of the towers. The colour of T2 includes a matte bronze-like tone, differentiated from the neutral palette of T1.

These matters are outlined further within the *WAM architectural documents*.

Podium.

(e) The Podium includes three separate buildings across the site (P1, P2 and P3) which provide the base of the overall development. Building P1 is at the base of Tower 1, P2 at the base of Tower2 and P3 is a separate component (collectively known as the Podium).

(f) The Podium buildings address the street frontages and frame an internal network of lanes and public space (Te Urunga Hau), connecting with the podiums of the existing adjacent buildings within the block (discussed further below).

(g) *P1 + P2*

Podium P1 and P2 are 7-storey building components located at the base of Tower T1 and T2, respectively. They include offices on levels 02 – 06 and retail tenancy spaces and lobbies on their ground floor. Like T1 and T2, their design also includes a glazed curtain wall design system across their façades to the street and internally to the Te Urunga Hau (refer **Image D** below). On the upper level of these respective buildings are open roof terraces.

(h) *P3*

Podium P3 is a separate building component on the northern boundary of the site. It is proposed to be two occupied levels (three level façade) and includes retail tenancy spaces on the ground floor and a food marketplace on Level 01. The façades of P3 include a brick module system which includes varying surface patterns and reflectivity treatment (refer **Image E** below).



Image D: Render of P1⁸ (lower right) and P2 (lower left).
(Sourced from the WAM Document D – Section 2.7).



Image E: Render of P3⁹ (lower left) and P2 (lower right).
(Sourced from the WAM Document D – Section 2.7).

⁸ Note, this image is for illustrative purposes only and indicates the outcome envisaged upon the Lower Hobson Street flyover removal. The flyover removal forms part of a separate consent process, and this is outlined within the AEE document.

⁹ *Ibid.*

Public Realm.

*Te Urunga Hau (the Urban Room).*¹⁰

- (i) The public realm design for the proposal includes Te Urunga Hau (the Urban Room) (refer **Image F** below). Te Urunga Hau provides a sheltered space and connection between the public realm of the downtown area and the internal laneways. Its design intent is to provide a connection between the land and the harbour, forming an additional public space to complement those along the waterfront, within downtown and the central city.
- (j) Te Urunga Hau is split across two levels, ground and Level 1, which provides connection through the site and access to the respective buildings¹¹.
- (k) The design includes a number of gathering / 'dwell spaces' within the laneways through site. Primary pedestrian entrances are from the northwest (Lower Hobson St) and the south (Customs St West), with secondary access provided from the east (Lower Albert Street) and southeast (corner of Lower Albert Street and Customs Street West).
- (l) The materials proposed include stone paving and steps, concrete planters and timber seating combined with a series of seating and table elements¹².
- (m) The planting proposed provides an organisational function and reflects the transition from the proximate coastal location to the whenua to the south.
- (n) The meaning behind the name of **Te Urunga Hau** builds on the vision for Te Pūmanawa o Tāmaki. Urunga is interpreted as an arrival place, landing or destination, while hau is both the winds, and a metaphor for communities¹³.

¹⁰ Refer Section 2.4 of the WAM Document D.

¹¹ Pedestrian, cycling and vehicles.

¹² Refer Section 2.4 of the WAM Document D.

¹³ *Ibid.*

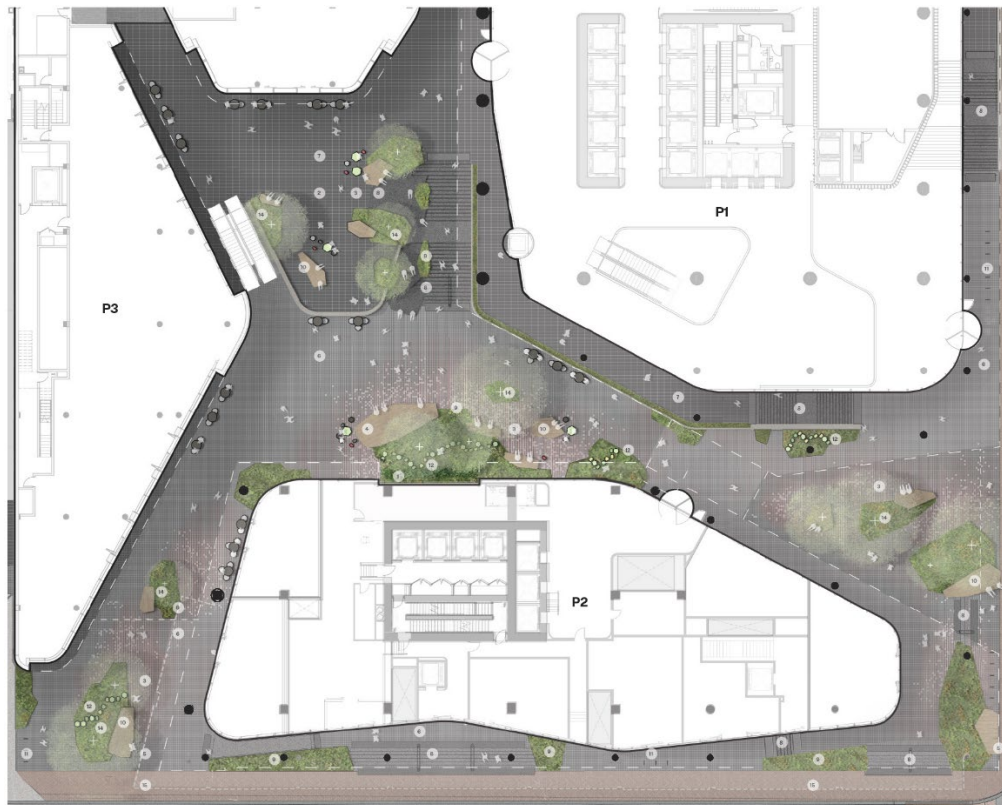


Image F: Indicative Landscape Plan of the Te Urunga Hau (not to scale).
(Sourced from the WAM Document D – Section 2.4).

- 21 Also included as part of this consent, the respective overbridges are to be demolished and removed. These are (i) pedestrian over Lower Hobson Street, and (ii) vehicular over Customs Street West (refer dashed outlines on **Image A** above).
- 22 The removal of Lower Hobson Street flyover and redesign of this area is not included as part of this resource consent application as it forms part of a separate consent process. The flyover is discussed further within the assessment of effects section below.
- 23 The changes to the design (as raised within paragraph 8 above) relevant to landscape assessment are summarised¹⁴ as:

Tower 1

- Change in use from residential to office results in amended floor to floor heights and slight adjustment to chamfer diagonal pitch lines.
- Reduction in plant level and louvres on the facade with removal of residential.

¹⁴ Refer B&A Planning Summary statement for full outline of the changes related to design development.

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- Updated lifting strategy and T1 core with no longer the need for a residential lobby and lifts, and therefore resulting in changes to the street edge.
- Custom Street stair moved west 4.5m to provide fire egress.

Tower 2

- Reconfigured apartment planning within T2.
- Update to T2 façades to reflect new internal planning (locations of balconies, solid, and glazed panels), however overall architectural intent maintained.
- Minor changes to diagonal pitch line of chamfers as a result of apartment planning.

- 24 The design on this site presents the opportunity to create a quality mixed use development within the western part of downtown Auckland, proximate to the waterfront and harbour edge.

PLANNING CONTEXT

- 25 A detailed description of the application's planning context (including the relevant statutory and non-statutory provisions) that provides the framework for assessing the proposal is provided in the AEE by Barker & Associates Ltd. This includes key provisions within the AUP and those related to Plan Change 78 (**PC78**).
- 26 The key provisions most relevant to **landscape** matters are set out in **Appendix C** of this report for the purpose of framing the landscape assessment (for the avoidance of doubt, the appendix does not include all relevant provisions and the assessment of the proposal against the provisions is provided in the AEE report). In summary, relevant provisions include the following:
- 27 The site is zoned *Business – City Centre* (refer **Image G** below) and sits within the *Downtown West Precinct* under the AUP.

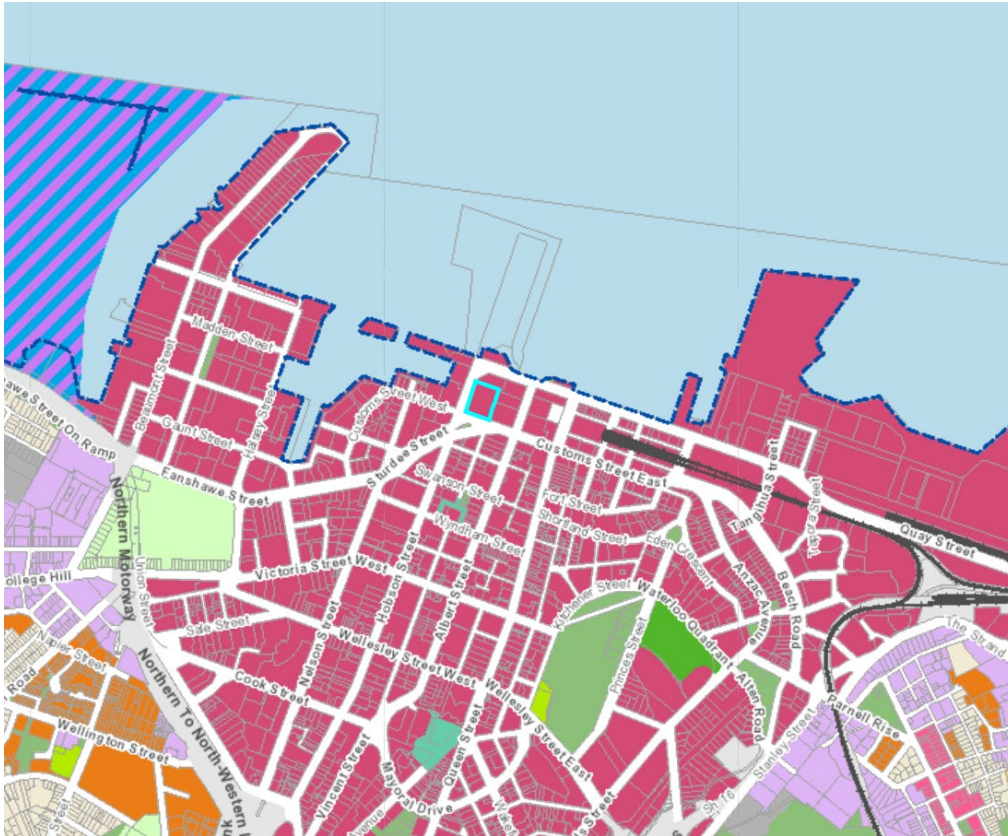


Image G: AUP zoning – approx. extent of site shown in light blue (not to scale).
(Sourced from the Auckland Council Geomaps Unitary Plan Maps).

- 28 The *City Centre* zone and the *Precinct* include a series of objectives, policies and assessment criteria which are relevant to the assessment of effects related to this proposal.
- 29 It is understood that the proposal is overall a *discretionary* activity¹⁵ as it is a new building which, in particular relation to landscape and visual effects, infringes¹⁶ the provisions related to:
- Harbour edge height control plane.* As illustrated on **Image H** below;
 - Tower dimension, setbacks from the street and separation;*
 - Pedestrian connections.* Pedestrian connection not provided at grade through the site for its entirety; and
 - Comprehensive development signage.*

¹⁵ This is due to any new building requiring consent as a *restricted discretionary* activity in both the *City Centre* zone and the *Downtown West Precinct*. The infringement to the *Harbour Edge Height Control Plane* is a *discretionary* activity. This is discussed further within the AEE document prepared by Barker & Associates Ltd.

¹⁶ An outline of the specific infringements to these provisions and other infringements / consent matters is provided within the AEE document prepared by Barker & Associates Ltd.

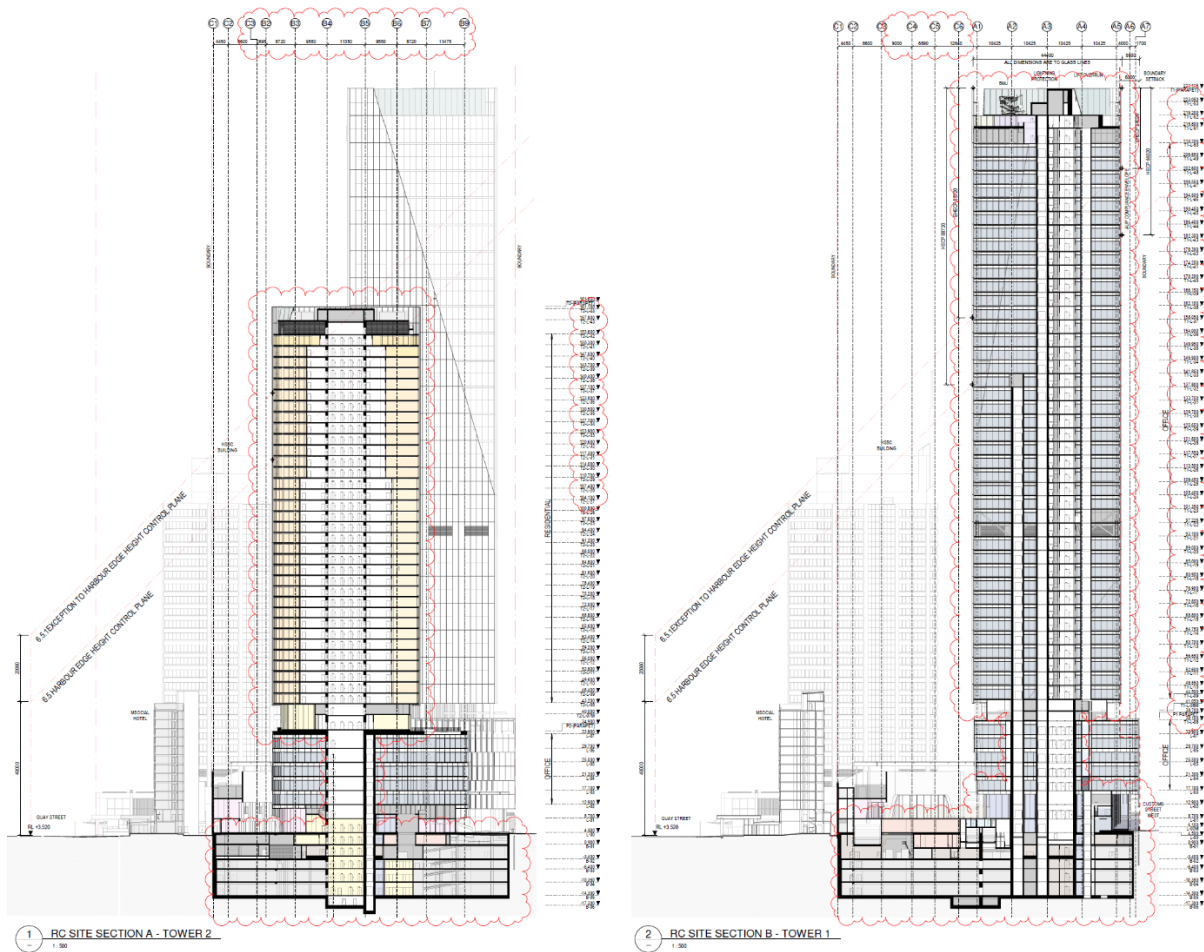


Image H: Indication of the AUP Harbour Edge Height Control Plane (not to scale).
(Sourced from the WAM Document A RC31-0001 Rev F).

30 **Objectives** of the *City Centre zone*¹⁷ and *Downtown West Precinct*¹⁸ relevant to the landscape assessment are summarised and paraphrased as follows, noting that the full provisions have been reviewed and are acknowledged in the assessment in the later sections of this report (emphasis added):

- A strong network of centres that are **attractive environments**,
- Development is integrated, contains a mix of uses and is of a **form, scale and design quality** so that centres are reinforced as focal points for the community,
- Positive contribution to the planned **future form and quality**,
- Development in the city centre is an attractive place to **live, work and visit**,
- **High quality** public open space is provided and **pedestrian connectivity**¹⁹ is enhanced,

¹⁷ Refer *H8 Business – City Centre zone* of the Auckland Unitary Plan (AUP).

¹⁸ Refer *I205 Downtown West Precinct* of the AUP.

¹⁹ This includes standard *I205.6.2* in relation to pedestrian connections.

- Development managed to accommodate **growth** and **intensity** while respecting the valley and ridgeline form and **waterfront setting**, and
- Maintenance and enhancement of **built** and **streetscape character** and amenity.

31 The relevant **policies** are summarised as:

- Development of a quality and design which provides positive contribution to the **visual quality** and **interest** of streets and public open spaces and **pedestrian amenity**. An **integrated** network of streets, lanes and pedestrian connections to publicly accessible spaces are provided,
- Requirement for large scale development of a design **quality commensurate with** the **prominence** and **visual** effects,
- A range of activities that enhance the **vitality, vibrancy** and **amenity** of the city centre... including a diverse range of activities with commercial and residential activities,
- Encourage specific outcomes in areas of the city centre that relate to **transformational development opportunities**,
- Enable **tall buildings** and the greatest **density** in the core central business district,
- Manage building **scale, height, form** and **design** to avoid adverse **dominance** and/or **visual amenity** effects on streets and public open spaces and encourage **well-designed slender** towers, and be complementary to the existing or planned character of precincts,
- Respect the **valley** and **ridgeline form** of the city centre, and
- Require **highest quality** building and development in the city centre, and building frontages to streets.

Harbour Edge Height Control Plane (HEHCP)

32 The purpose of the **harbour edge height control plane**²⁰ is to manage the scale of buildings at the western end of Quay Street to:

- Provide a **transition in building height** from the core central business district to the waterfront,
- Maximise **views between the harbour and the city centre**, and
- Reinforce the **Quay St east west connection** running from the corner of the Strand and Quay Street to the east and Jellicoe Street in Wynyard Precinct to the west by the **alignment of tall building frontages**,

²⁰ Refer *H8.6.5* and *H8.6.6* of the AUP.

- 33 While the proposal is a discretionary activity, **assessment criteria** listed for consideration by way of restricted discretionary activity for exceptions to the **HEHCP** are related to the following:²¹
- a) *visual profile,*
 - b) *waterfront amenity,*
 - c) *streetscape and street corners,*
 - d) *surrounding properties,*
 - e) *design of the upper parts of the building, and*
 - f) *particular constraints.*

Tower Dimension, Setback from the Street and Tower Separation

- 34 Key provisions in relation to the maximum **tower dimension, setback from the street and tower separation**²² are summarised as building which:
- are **not overly bulky** and are **slender in appearance**;
 - provide adequate sunlight access to streets;
 - provide a consistent **human-scaled edge to the street**;
 - provide adequate sunlight and outlook around buildings;
 - enable views through the city centre; and
 - mitigate adverse wind effects.
- (1) On every site identified as **special height area** on Map H8.11.3
 - (a) the maximum plan dimension of that part of the building 28m above mean street level must **not exceed 50m**; and
 - (b) the part of a building above 28m must be located at least **6m from all boundaries** of the site.
- 35 The **matters of discretion**²³ for new buildings include:
- Building **design** and **external appearance**,
 - Effects of **additional building scale** on neighbouring sites, streets and open spaces (sunlight access, **dominance, visual amenity** and **landscape character**), and
 - Consistency with **planned future form** and **character**.

²¹ Refer *H8.8.2.(8)* of the AUP. These are expanded on within **Appendix C** and assessed within the *Assessment of Effects* section below.

²² Refer *H8.6.24* of the AUP.

²³ Refer *H8.8.1* of the AUP.

- 36 Relevant **assessment criteria**²⁴ within the AUP relate to:
- Building **design** and **external appearance**,
 - Contributing to a **sense of place**,
 - The **silhouette** of the building **contributing** to the city centre's **skyline**,
 - Creating a **positive frontage**,
 - Providing **variation** in building form / visual interest,
 - Managing effects due to the infringement of building height in relation to **scale, dominance, visual amenity** and **character**, and
 - Consistency with planned **future form** and **character**.

*Pedestrian connections*²⁵

- 37 On the subject site²⁶, any new building is to provide **at-grade**, east-west and north-south **pedestrian connections** across the block. The purpose of these provisions is to support (i) the transport interchange in the area and facilitate clear, unobstructed pedestrian movement, and (ii) pedestrian movement between the City Centre Core and the waterfront.
- 38 Specific standards include the provision for (i) straight pedestrian routes with a width of at least 5 metres, (ii) weather protection, (iii) natural daylight (glazed canopies), (iv) public access, (v) active uses at ground level, and (vi) general alignment with Federal Street (the north south connection).

*Comprehensive Signage*²⁷

- 39 The relevant **objectives**²⁸ are summarised as (emphasis added):
- Billboards and comprehensive development signage are managed to maintain ... **visual amenity** values of buildings and the surrounding environment.
- 40 The relevant **policies** are summarised as:
- Enable billboards and comprehensive development signage while avoiding signs creating **clutter** or **dominating** the building or environment by controlling the size, number and location of signs; and
 - Manage the effects of billboards and comprehensive development signage to maintain the values of scheduled historic heritage places and **visual amenity** values.

²⁴ Refer *H8.8.2* of the AUP.

²⁵ Refer *I205.6.2* of the AUP and **Appendix C**.

²⁶ The block bounded by Lower Albert Street, Customs Street West, Lower Hobson Street and Quay Street.

²⁷ Refer *E23* of the AUP and **Appendix C** which outlines the assessment criteria.

²⁸ In relation to landscape assessment matters.

EXISTING ENVIRONMENT

- 41 The following section provides a description of the existing environment and attributes relevant to the landscape assessment. This is in relation to (i) the site and the localised streetscape environment, and (ii) the broader skyline (cityscape).
- 42 The site is located in downtown Auckland on part of the reclaimed land within the city seaward of Fanshawe Street. It is within a key waterfront block for the city which is proximate to the harbour edge but is setback by Hobson Street and buildings within Viaduct Harbour (west) and existing buildings fronting and along Quay Street and on Princes Wharf (north). The area has distinct characteristics including flat topography and a street grid which is interrupted on its inland side by Sturdee and Fanshawe Streets which follow the alignment of the former shoreline.
- 43 Through recent improvements to the city centre, such as the Te Wānanga open space, the Quay Street cycleway and development of Commercial Bay there has been an increased connection and appreciation of Auckland's harbour.
- 44 The central city has two recognised axes:
- The harbour edge axis along Quay Street (east-west), and
 - The city-to-harbour axis along Queen Street (north-south), to which Hobson Street and Albert Street run parallel.
- 45 The site is located in a space between the two axes, west of Queen Street and between Quay Street (north) and Customs Street (south). The site's location is at the 'western corner' of the core central business district.

The site and localised context²⁹

Subject site and city block

- 46 The site is within the centre of a city block which is bounded by the wide streets of Lower Hobson Street to the west at 32m wide, Customs Street West to the south at 30+m wide (when considering the triangular street arrangement with Sturdee and Fanshawe Streets), Lower Albert Street to the east at 27m wide, and Quay Street to the north at 30+m. It is setback from the harbour by existing development within the Viaduct (west) and Princes Wharf and development to Quay Street (north).

²⁹ Refer to the following sections of the WAM *Document D*: **1.1 Application Scope**, **2.1 City Connectivity & Context** and **2.2 Public Realm** which provide an outline (and diagrams) of the site and its central city context.

- 47 The site is currently occupied by the 8 storey DTC building which occupies the majority of the site. On its western frontage at street level is the 'Pointers' bar and 'Monsoon Poon' restaurant. These businesses provide activation of the street edge but within a low amenity setting. There is also a row of dishevelled and overgrown trees which do little to add to the environment. On the building's southern side (again at street level) is a wide opening into the building which provides vehicle access into the DTC and also the service lane for this city block³⁰. These openings also do little for the streetscape amenity and provide a poor pedestrian environment.
- 48 To the east within the same city block are the existing HSBC Tower and AON House high-rise buildings which front and define the street edge to Lower Albert Street³¹. On the northwestern side of the block (fronting Quay Street) is the 13 storey M Social hotel building. It is setback from Quay Street within the block to allow for vehicle movement and access. The M Social is a 'slab' building with a single, north-facing frontage (corridors run along the southern side of the building) and it effectively 'sleeves' the DTC building.

Streetscape environment

- 49 The existing streetscape environment within the localised context of the site has very low amenity values. This is largely due to the vehicle dominated environment (connections through the area, complex road configuration, access to / from the DTC) and also the form and presence of the Lower Hobson Street flyover. The flyover provides vehicle connection from Quay Street to Hobson Street but creates a poor streetscape environment in the area. This is further exacerbated by the presence of the overhead vehicle 'exit' from level 3 of the DTC across to Fanshawe Street. There are however a number of established street trees at the junction of Sturdee Street and Customs Street West, but they are located within the 'pinch' between the retaining wall³² and the DTC building. This retaining wall creates division and lack of connection to the south.
- 50 To the east, Lower Albert Street provides an interface and connection to the street and across to Commercial Bay and the greater city beyond. There is currently an indirect stair and walkway on this frontage which connects into the site.
- 51 While the site's location is within a key waterfront block between the CBD and Viaduct Harbour, it suffers from:

³⁰ This lane services all buildings within the city block and provides a north-south vehicle connection between Quay Street and Customs Street West.

³¹ Their location is illustrated on **Image A**, earlier within this report.

³² Which provides a level change between Fanshawe Street and Sturdee Street.

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- Poor street frontages, with inactive edges around much of the block, poor pedestrian amenity on footpaths;
- Poor amenity resulting from the Hobson Street flyover / ramp on the western side of the block and the overhead car parking ramp over Customs Street West;
- Poor through-block connections;
- Interrupted connections to the south into the balance of the city because of the retaining wall which runs parallel with Fanshawe Street; and
- Undistinguished architecture (especially the DTC and M Social hotel).

City form and topography (and broader cityscape)

- 52 Within the area, the city form has a mix of contemporary and heritage buildings which largely increase in height stepping back from the harbour edge and reflect their location and the underlying topography.
- 53 As previously outlined, the site is located proximate to the harbour edge on reclaimed land. It is aligned with the Federal Street ridge (north-south), just to the north of a former headland between Commercial Bay and the large bay to the west now occupied by the Viaduct Harbour. It is opposite a former headland pā Ngā u Wera and was also formerly known as Point Stanley³³.
- 54 Tall buildings on Auckland's skyline are predominantly located in the central city on either side of the Queen Street valley, which recognises and reflects the topography and 'ridge and valley' pattern as identified by the AUP provisions. In this context the site is located on the western side of the city and will therefore be more prominent from the west. Notably however, the 'ridge and valley' pattern is only really evident from viewing locations to the north of the city, not from the east and west where the majority of views are afforded. Also, consequently, the Special Height Area of the AUP (which provides for the tallest buildings) is centred on the Federal Street ridge - as is the Sky Tower.
- 55 The site, and block within which it is located, is therefore relevant to north-south topography and urban form, and to the east-west waterfront connection between the CBD and Viaduct Harbour. The present buildings and conditions within the subject block are poor in both respects.
- 56 The building form within the city centre along Quay Street includes a number of larger buildings built to the street edge (the HSBC Tower and the Deloitte Centre) and also a series of mid-range scaled buildings such as the M Social Hotel, the ICBC Building and the buildings which span between Te Komitanga

³³ <https://www.aucklandcouncil.govt.nz/arts-culture-heritage/heritage-walks-places/Documents/auckland-city-heritage-walks-original-shoreline.pdf>

(Lower Queen Street) and Britomart Place³⁴. Also along this extent there are smaller 2 – 3 storey buildings which front the street such as the Northern Steamship Building. The PwC Commercial Bay tower is one of the tallest buildings (within the block that fronts Quay Street) and it is setback from the northern Quay Street edge.

City connectivity

57 Although on the north-western corner of the central city, the site is close to the city centre facilities (and the proposed development will contribute to the activities) including business, retail, entertainment, education and open space. These include:

a) It is within a 2 – 10 minute walk to:

- Britomart Transport Centre,
- Ferry Terminal,
- The bus terminus on Lower Albert Street,
- Two future CRL stations - Waitematā (Britomart) and Te Waihorotiu (Aotea) (under construction),
- The Fanshawe and Customs Street East-West *Transit Street* (as per CCMP,
- Commercial Bay Precinct,
- Queen Street retail and business area,
- Quay Street Waterfront, Te Wananga (waterfront public space),
- Viaduct Harbour, and
- Tepid Baths.

b) Within a 10 – 20 minute walk to:

- A large portion of the balance of the central city,
- Wynyard Quarter and Silo Park,
- Victoria Park and Albert Park,
- Auckland University and AUT,
- Auckland Art Gallery, Town Hall, the Civic and Aotea Centre,
- The central city's pedestrian and shared streets network, including Fort Lane, Jean Batten Place and O'Connell Street, and
- Spark Arena.

³⁴ Some of which are 7 – 16 storeys.

- c) There is ready access to the cycle network within the central city, including along Quay Street and Nelson Street.

58 There are also a number of consented buildings in the city, some of which are under construction. These are illustrated on **Image I** below and within the Visual Study document (refer **Appendix D** to this report).

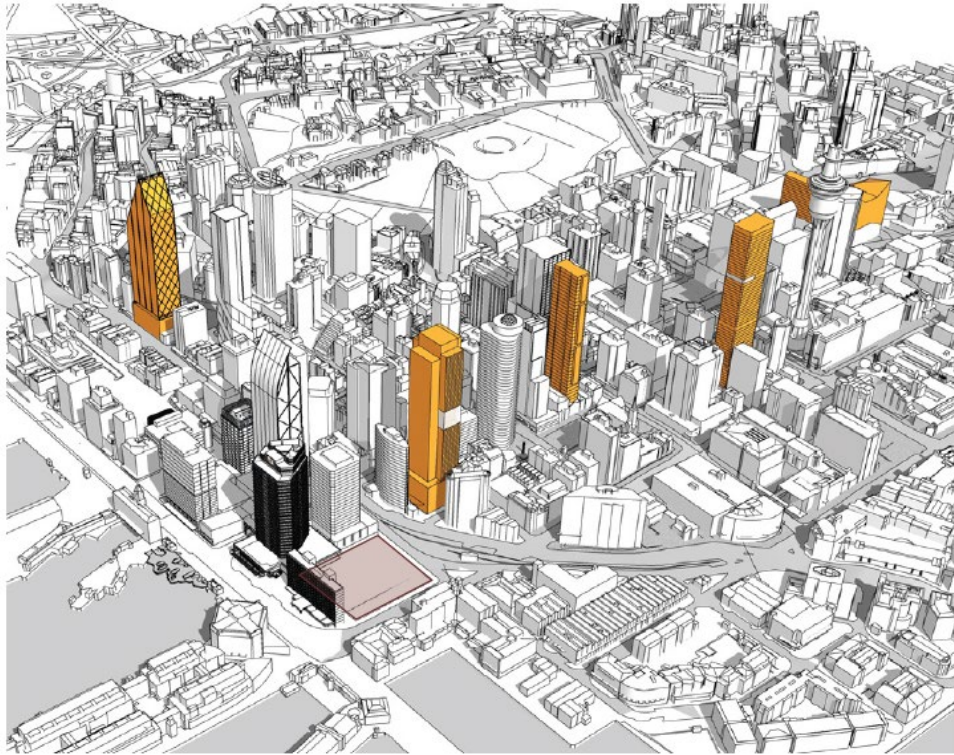


Image I: Illustration of the city model context (prepared by WAM) which includes buildings which are consented or under construction (orange). Note alignment to the Federal Street ridge. (Sourced from the WAM Visual Study Section 1.1 – refer **Appendix D** to this report).

ASSESSMENT OF EFFECTS

59 The following section assesses the proposal and its level of potential effects and appropriateness in this setting related to landscape assessment matters. Matters to be addressed relate to the **urban landscape** context and **visual amenity**, and reflect the key provisions related to the *Downtown West Precinct* and *City Centre* zone. These are summarised and addressed related to following:

- a) Potential effects on the **streetscape** (podium scale and appearance, street-level circulation, and activation);
- b) Potential effects on the **broader cityscape** (scale, appearance and context of the towers on the skyline); and
- c) **Appropriateness** and effects of the proposal in its urban landscape context.

- 60 A series of panoramic photos and visual simulations³⁵ illustrating the proposal and its surrounding context are in **Appendix B** to this report. The viewpoints³⁶ have been selected as they represent public views toward the site from a range of locations within the surrounding and broader landscape context³⁷. They are referred to within this assessment of effects section of the report, as necessary.
- 61 The aforementioned **urban design assessment**³⁸ provides specificity on design matters from an urban design perspective. These reports are complementary and there is overlap across the matters assessed. The areas of overlap include assessment of:
- a) The streetscape environment and frontages in terms of urban and built form, connectivity and activation,
 - b) The scale, design and appearance of the Podium and Towers (including variation and differentiation),
 - c) Harbour Edge Height Control Plane (HEHCP) provisions,
 - d) City Centre Masterplan (CCMP) context,
 - e) Te Urunga Hau public space,
 - f) Public realm, lanes and circulation,
 - g) Signage.
- 62 The key areas of difference between the assessments includes:
- a) Visual amenity from the broader cityscape context,
 - b) Specificity of the maximum tower dimensions and setbacks,
 - c) Shading,
 - d) Parking.
- 63 A key matter related to this proposal is the scale of the towers in this location when assessed against the AUP provisions and standards, and the urban form context. This includes the HEHCP standard which this proposal breaches. However, the site is in the special (unlimited) height area where the tallest buildings are anticipated. The assessment below takes this into account.

³⁵ Prepared to evidential standard.

³⁶ Viewpoint locations are consistent with those suggested by Auckland Council's landscape specialist. Refer visual simulations as **viewpoints 01 – 16 (Figures 2 - 33)**.

³⁷ Refer **Viewpoint Location Plan (Figure 1 within Appendix B)**.

³⁸ Written by McIndoe Urban included as part of this resource consent application (an also updated to reflect the amended design).

- 64 It is also key to outline that change itself is not an adverse effect. The city's planned urban form envisages change, and the key matter is whether the building will contribute to the city's planned urban form.
- 65 For the avoidance of doubt, this assessment is provided with the Lower Hobson Street flyover retained. Notwithstanding, assessment commentary is provided in certain sections below where the flyover is removed.

Potential effects on the streetscape (podium scale and appearance, street-level circulation)

- 66 The proposal will provide a new element within Auckland's wider cityscape and skyline and will also have influence on the streetscape environment. It will contribute to and provide a positive streetscape interface with an appropriate scale and appearance, and enhance the character of the area³⁹, particularly with Hobson Street and Customs Street West.

Podium form

- 67 The proposed podium buildings (P1, P2 and P3) will have an attractive appearance to the street frontage. Their design will define this corner site and help rejuvenate a dilapidated area.
- a) The podium buildings being three (P3) and six storeys (P1, P2) will present an appropriate street frontage which has a human scale and is proportionate to the scale of the surrounding streetscape environment;
 - b) The façades across P1 and P2 have been designed to present an architecturally coherent scheme through their form, scale and materiality;
 - c) The buildings have variation to their form in both elevation and plan which provides interest to the street, but also generous pedestrian access and space;
 - d) The buildings occupy the majority of the street frontages and provide activities on their ground floors and glazing to provide passive surveillance to the respective streets;
 - e) The materials are of a high-quality and provide visual interest to the respective façades. This is achieved through their horizontal and vertical proportions and the subtlety to the setback of the glazing components, which helps break up the mass and provides shadow and depth;
 - f) The patterning on the façades is consistent but slightly varied across each level of P1 and P2 through the spacing and offset of the vertical elements. This provides depth and rhythm;

³⁹ Refer to the 3D rendered views of the proposed streetscape environment within Section 2.7 **Streetscapes** within the WAM *Document D* included as part of the application.

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- g) The *bronze-like* tone of the solid elements on the podium façades will add warmth to the buildings (and the development overall) at ground level, providing differentiation when compared to the 'lighter' towers above;
- h) P3 has a different façade treatment to P1 and P2 with the use of brick. It does not have the same extent of street frontage as P1 and P2 and its façades predominantly face internal to Te Urunga Hau. The use of brick on P3 however is consistent with the façade treatment of P1 and P2 (on their internal frontages) and will assist with legibility and help draw pedestrians into the site's laneway network.

68 In terms of materials, the podiums use a mix of glass and brick. The brick used on the podiums will also be used to retrofit the existing podium of the AON building which enables consistency and coherency. In particular, the brick will signal entrances to the interior of the block from three of the perimeter streets (Lower Hobson Street, Customs Street West and Lower Albert Street) and will continuously signal the main east-west route through the block between Lower Albert Street, Te Urunga Hau across to Lower Hobson Street.

69 The lower height and different treatment of P3 (compared to P1 and P2) will contribute to the organic character of Te Urunga Hau and will allow greater levels of sunlight into the space.

70 The podiums will be differentiated by the towers above by:

- a) The setbacks provided to the street edge above Level 7. Although this is less than the AUP standard, the setback will reduce potential adverse dominance effects within the streetscape environment;
- b) The chamfers and chiselled edges at the base of each of the towers will give strong shadow between podium and tower and result in visually interesting buildings;
- c) The rooftop garden terraces / open space on the podium roofs will provide a visual 'break' between these elements; and
- d) The different façade treatment across the podiums and towers, although there is enough commonality that they are aesthetically coherent.

Street Activation and Circulation

71 The proposal will increase activation of the street edges, providing connections into the site with the wider cityscape. This is achieved through the ground level occupation of the buildings which includes retail and food and beverage spaces and also lobby entrances providing access into the offices and residential apartments.

72 The wide openings on both Customs Street West and Lower Hobson Street provide legible connections and access into the site and the public space activities of Te Urunga Hau (Urban Room). The connection to Lower Albert

Street provides enhanced and legible east-west connections into the site and the wider city. The activities proposed will assist with supporting circulation to and through the site in the following ways:

- a) Retail and lobbies located to these frontages or within the site (within Te Urunga Hau) to provide activation and draw people into the site;
- b) The consistency and coherency of the materials and design language used across the respective spaces (such as street edge, buildings, canopies, and within Te Urunga Hau). This is consistent and provides a visual cue that will contribute to legibility of the through block connections;
- c) Pedestrian connections are legible and offered through the site to connect with other parts of the city and also to the HSBC tower and AON house. To this end, north-south and east-west connections are provided between the AON House and T1;
- d) The new stairs on Lower Albert Street directly front the street on the alignment of the existing internal east-west lane between AON House and the HSBC tower (connection to this lane is currently via a circuitous route). The splay on the stairs orients the northern side of the stairs to Wheriko Lane which spans through Commercial Bay. This provides an improved visual and physical connection between the site and Commercial Bay; and
- e) The location of the service lane (from Customs Street West) is logical as it utilises an existing access way.

73 Per the *Downtown West Precinct* provisions within the AUP⁴⁰, 'at grade' pedestrian connections are sought east-west and north-south through this development block. However, it is not possible to create these connections at grade because of existing development (namely the M Social, HSBC and AON buildings).

74 While the proposal provides internal lanes and at-grade connections to Customs Street West and Lower Hobson Street, it also includes measures to connect with the existing 1st floor circulation around the HSBC and AON buildings in east-west and north-south directions, and to Lower Albert Street. The design includes measures to enhance those existing podia (paths around those buildings) through new stairs at the respective street frontages, consistency of materials (notably use of brick) which contributes to legibility of circulation, canopies, planting and activation of the lanes with retail use and activities. This will enhance the experience and connections, and improvement in amenity and activation of the site will also improve circulation around the perimeter streets.

⁴⁰ Refer I205.6.2 of the AUP.

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- 75 The connections across the change in level within the site is successfully achieved by the escalator and stairs in combination with the other open space qualities within Te Urunga Hau. The necessary change in level results in a positive feature of this internal public space.
- 76 The dimension of the pedestrian connections provided is in excess of the minimum 5m width sought within the provisions. The connections at the street edge are splayed which provides generous, legible and obvious connection, and allows for direct pedestrian movement into and through the site. The glass canopy above Te Urunga Hau will provide weather protection and a feature of the design.
- 77 Although a north-south connection is provided through the site (between T1 and AON House), a major constraint to providing a better connection from the site through to Federal Street (to the south) is the level difference and the existing retaining wall between Fanshawe Street and Customs Street West (south of the site).
- 78 **Images J – M** below illustrate the proposed street edges. ⁴¹



Image J: Render of the proposed activation of the Lower Hobson Street and Customs Street West frontage. (Sourced from the WAM Document D – Section 2.7).

⁴¹ Additional images are provided on pages 32, 34, 36 – 38 within **Section 2.7 Streetscapes** of the WAM Document D.



Image K: Render of the Custom Street West frontage, including access to the residential lobby and service lane into of T1. (Sourced from the WAM Document D – Section 2.7).



Image L: Render of the proposed activation of the corner of Lower Albert Street and Customs Street. (Sourced from the WAM Document D – Section 2.7).



Image M: Render of the proposed activation of the street frontage on Lower Albert Street (east-west connection through the site). (Sourced from the WAM Document D – Section 2.7).

Public Space / Te Urunga Hau (Urban Room)

- 79 The Te Urunga Hau (Urban Room) will provide an attractive and inviting public space as part of the wider development. It includes a series of covered gathering and meeting places in its centre which are framed and defined by buildings P1, P2 and P3. These edges are activated with the inclusion of retail and lobby entrances into the respective buildings. Refer **Image N** below.
- 80 On the street edges there are legible pedestrian entrances and connections into this internal space. This is achieved through their openness, visual permeability and consistency of materials. The inclusion of larger tree species will also help define the space.
- 81 The design of Te Urunga Hau, overall, is coherent and generous in size and will include a series of smaller spaces which have been arranged to enable places to meet, gather, rest or play.
- 82 Through site connections are achieved with access provided to / from Customs Street West, Lower Hobson Street and Lower Albert Street. The connections allow direct movement into and through the site into the centre where there is a 'convergence' zone where the meeting and gathering spaces are located.
- 83 A complete north-south connection is not possible across the block due to the location of the M Social building (north of the site). However, an improved link is provided with the northern reaches of Lower Hobson Street and also to and across the podium of the HSBC Tower which, in turn, connects to Quay Street.
- 84 The proposed connectivity through the site is a significant improvement on the existing environment as there is no real connection, or one that is easily attained, through the DTC building. The east-west connection to Albert Street also provides a vast improvement and will allow clear and obvious connections between the site and Commercial Bay to the east.
- 85 A change of level on the through-block lanes is unavoidable given the existing constraints of the HSBC and AON buildings, and the servicing and car parking associated with those buildings at ground level. The proposal will tie in with the existing conditions to provide a coherent network of internal lanes and public space. The stairs are used to enclose one side of Te Urunga Hau and also provide an overlook of the public space. It turns the necessary change in level into a feature that will contribute to the character of Te Urunga Hau.



*Image N: Render of the Te Urunga Hau (Urban Room).
(Sourced from the WAM Document D – Section 2.4).*

Nearby Street Views

- 86 The proposed development of the podium, public realm space and towers will present a significant landmark for the lower downtown area and for the city as a whole. As outlined earlier it will revitalise and transform this part of the city whilst also providing a significant public realm design with activation of edges and streets connections to pedestrian laneways.
- 87 **Images D, E and J – M** above illustrate the proposal from locations in close proximity to the site on Lower Hobson Street, Customs Street West and Lower Hobson Street⁴². They show the proposal from the respective streets, at intersections and key junctions and access points into the site.
- 88 These images illustrate:
- a) The street interface and human scale of the street frontages;
 - b) The design of the podium buildings and the interest and variety of the respective façades;
 - c) The height and variety of the ground floor and how it addresses the street and ‘opens up’ to the pedestrian connections into the site;
 - d) The legibility of entrances (such as lobbies); and

⁴² Additional images can be found within **Section 2.7** of the WAM Document D.

e) The distinction between the podium and the towers. Although they have a coherent and complementary design, the respective towers are setback from the podium edge to provide visual separation.

89 The localised streetscape environment on Quay Street (north of the site) is fronted by the 13 storey M Social Hotel and the HSBC tower, which define the street edge. Proposed tower T1 is setback from the Quay Street boundary by 74.8m and T2 by 47.8m, which provides distance and separation to these buildings. From this location on Quay Street the lower parts of the respective towers will be screened with only the taller elements visible when one was to look up, with the main focus of the view being of the localised streetscape environment. The setback and stepped nature of the proposal (from T2 up to T1 in the context of the other aforementioned buildings) will reduce any perceived dominance and has also been appropriately mitigated through the design of the buildings (such as form, façade treatment).

Signage

90 In relation to **signage**⁴³, the proposal includes ‘signage zones’ which indicate the location of future signs (which will be refined post resource consent). The signage includes shopfront signage for the respective tenancies, and Precinct, Podium and Tower naming rights and locations⁴⁴.

91 From a landscape assessment perspective the ‘signage zones’ indicated are appropriate as they are located in logical, defined and contained locations which are integrated into the respective façades of the buildings. Potential visual clutter and cumulative effects are avoided to the streetscape environment given the limited number of ‘zones’ and the consistent and defined locations indicated for signage. They will not detract from the form and appearance of the buildings and will not be dominant elements.

Summary of Streetscape

92 The proposal will revitalise and transform this part of downtown Auckland. It will replace the car parking building with an attractive and architecturally cohesive development. Any potential adverse effects on the streetscape have been addressed through the design which has an appropriate form and human scale to the street and qualities which provides visual interest. It also provides a mix of activities to the street and within the Te Urunga Hau (Urban Room) which will help activate the street and site, proximate to the waterfront. It will provide pedestrian connections into and through the site’s lanes which supports movement between the city centre and the waterfront.

⁴³ Refer **Appendix C** and *E23* of the AUP.

⁴⁴ Refer **Section 2.10** of the WAM *Document D*.

- 93 The removal of the Lower Hobson Street flyover would help to fully realise the positive effects of the proposal on streetscape amenity, activation, and pedestrian circulation. However, even if the flyover is retained (which is the basis on which this assessment has been carried out), the proposal will present a significant improvement to the streetscape environment and would have positive effects.

Potential effects on the broader cityscape (scale, appearance and context of the towers on the skyline)

- 94 The proposed towers within the development will provide new elements to Auckland's cityscape and skyline. This section of the report discusses the design of the towers and the wider views of the proposal. They will contribute to the cityscape and the site's location proximate to the harbour in the ways as outlined below.
- 95 The proposal will continue the tradition in central Auckland city with buildings built along ridgelines and respecting the "valley and ridgeline form of the city centre..."⁴⁵. The site is located proximate to the harbour edge on reclaimed land, aligned with the Federal Street ridge (north-south) just to the north of a former headland and to the west of the catchment of Te Wai Horotiu awa (which aligns to Queen Street). The towers will be seen in the context of the emerging built form (some consented and under construction) within this part of the city with greater height centred on the Federal Street ridge as enabled through the AUP provisions in the downtown and central city area⁴⁶. The consented buildings are illustrated on **Image I** above and within the Visual Study document which form **Appendix D** to this report.
- 96 It is worth noting that change in itself is not an adverse effect. The city's skyline has changed considerably over the years and the AUP anticipates such change will continue, including those buildings already consented but not yet built. In fact, an evolving skyline can be seen as a mark of a vibrant city.
- 97 The site is located in the north-west corner of the 'core' central city (CBD), the north-west corner of the AUP 'Special Height Area', and the western end of Quay Street. Its location and the proposed buildings reinforce the alignment of taller buildings adjacent to or behind Quay Street (to the north) and its location at the western end of this frontage. The buildings provide something of a 'bookend' form for the city, providing a step and transition to the harbour edge and the Viaduct to the west (which can be seen on **Image O** below).⁴⁷

⁴⁵ Refer *Section H8.6.2* of the AUP.

⁴⁶ Refer *Map H8.11.3* of the AUP.

⁴⁷ Note, 'bookend' is not a concept anticipated by the AUP, however it provides a clear description of the how the proposal will sit within the city.

Noting also that where the towers will sit on the city skyline is dependent on the location and direction of view.



*Image O: Viewpoint 7 (thumbnail) Isthmus. Stanley Point
(Refer to Appendix B)*

Tower design and appearance

98 This section of the report discusses the appearance of the towers from a landscape assessment perspective. It provides context to the viewpoint assessment provided below. The following comments are made in relation to the design:

Slenderness

- a) The towers will be tall and **slender**. This is achieved through their overall form and their proportions; height helps with the appearance of slenderness;
- b) Although T1 is 55.9m in diagonal dimension and T2 is 50.6m (both in breach of the AUP standard H8.6.24.(1)(a), the chamfers on the upper levels reduce the 'diagonal' at those upper levels and helps with the appearance, emphasising the slenderness;
- c) The narrower dimension of the respective towers is the northern and southern façades which face the central city and harbour, whereas the wider eastern and western façades of T1 are partly internalised to the block;
- d) With regard to the eastern and western façades of the respective towers, these elements also have slenderness relative to those to the north and south. Although wider than the northern and southern façades, the slenderness is emphasised given the height and proportions of the respective buildings, their chamfered / chiselled form and the façade treatment. This includes the vertical glazing elements which expresses their verticality and provides depth and shadow, breaking up any apparent massing. It also varies dependent on the location and direction of the view toward the proposal;
- e) In combination with the chamfers on the upper levels, the material treatment of the façades helps emphasize the vertical lines and elements.

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This is achieved through the curtain wall with aluminium fins in combination with the fritted glass and glazing elements which span across two floors;

- f) The vertical elements and fins also provide depth and shadow to the façades and provide differentiation between the top and bottom of the towers.

Differences but likeness

- g) The towers have the same architectural language with irregular shapes, but they have been designed as complementary, slender forms which read as a pair or 'siblings within a family';
- h) Although similar, the colour of T2 includes a matte bronze-like tone, differentiated from the neutral palette of T1;
- i) The differences include their footprint, the size and orientation of the chamfers, the façade treatment and their staggered arrangement on the site. They are purposely offset from each other.

Sculpted form

- j) As well as contributing to slenderness, the chamfers on the respective towers will give each of the upper levels an asymmetrical form. This will contribute to a distinctive and 'crafted' skyline (i.e. as a form crafted by hand, water or light and giving an 'organic' quality). This is reflective of the design intent of the iho / design statement⁴⁸;
- k) The 'edges' of the chamfered form of the upper levels are anticipated to be lit at night. This will strongly contribute to the skyline of the city emphasising the carved form of the respective towers.

Tower relationship to the other

- l) The buildings will 'move' relative to each other as people move and view them from around the city. The family likeness, building offset, their slenderness and asymmetrical design will draw attention to this characteristic. Also, the slender space between the buildings will also open and close depending on viewing angle.

Tower relationship with the podium

- m) The design of the towers will complement the podium. The material composition and language used is similar to that of podium. Although having different building forms (the podiums being lower and wider, and the towers as taller and slender) the materials and their arrangement ensures they read as a coherent development. Both the towers and the

⁴⁸ Refer Section 1.4 Cultural Narrative & Hierarchy within WAM Document D.

podium components utilise the glazed unitised curtain wall system and in combination with the glazing provides verticality, interest and similarity. This is illustrated on the visual simulations (refer **Appendix B**) and on **Image P** below;

- n) The narrative of the design has informed the lighter form of the upper levels through the chamfered edges, lighter colours and materials, and glazing. This is subtly contrasted by the composition of the lower levels of the towers and podium elements which are more ‘grounded’ with chamfered edges, and darker colours and materials;
- 99 These qualities in combination will enhance the contribution of the two towers to the cityscape and present an interesting profile on the skyline.
- 100 The form and stepping of the towers between T1 and T2 serve to define the western edge of the central city and provide a transition between the core CBD and the harbour edge. This is discussed further, later within this report .
- 101 In terms of the relationship of the proposal to existing buildings along the Quay Street frontage and ‘harbour edge’ of the city, a transition in height is provided to the south. The proposal is setback from the Quay Street frontage (behind the M Social Building) and steps up to T2 and then T1, providing a transition consistent with the existing step in building form along the Quay Street east-west alignment. As illustrated on the ‘East-West Quay Street (South)’ cross section⁴⁹ the existing built form along the Quay Street edge is predominantly low-midrise buildings, with development potential not realised. In terms of the relationship and potential effects on the PwC Commercial Bay tower, the proposal will complement that building in terms of design and also their place within the city on the harbour edge.

⁴⁹ Refer to the ‘RFI Item 54 City Form Sections’ prepared by WAM– see page **61** of the updated WAM *Document D*.



*Image P: Render of P1⁵⁰ (right) and P2 (left) and the protruding T1 and T2.
(Sourced from the WAM Document A R80-0008 Rev D).*

Visual Amenity (views from the wider cityscape)

102 A series of viewpoint images and visual simulations⁵¹ are provided within **Appendix B** to this report which illustrate the proposal and views of the Auckland city centre from various locations around the broader cityscape. These locations include:

- a) Within the city centre.

⁵⁰ Note, this image is for illustrative purposes only and indicates the outcome envisaged upon the Lower Hobson Street flyover removal. The flyover removal forms part of a separate consent process, and this is outlined within the AEE document.

⁵¹ Prepared by Isthmus to evidential standard. Refer **Figures 1 – 33** within **Appendix B**.

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- St Patrick's Square, Queens Wharf, Quay Street, the Viaduct Esplanade, Karanga Plaza, Brigham Street (Wynyard Point) and the Hobson Street / Fanshawe Street intersection;

b) Wider city.

- **North:** Stanley Point (Cyril Bassett VC Lookout), Takarunga Mt Victoria;
- **East:** Ōkahu Bay Wharf (Ōrākei) and The Strand, Parnell;
- **South:** Maungawhau (Mt Eden), Ponsonby Road; and
- **West:** Shelley Beach Road overbridge, St Mary's Bay Beach, Sulphur Beach Reserve and Harbour View Beach Reserve, Te Atatu Peninsula.

103 Below commentary related to these wider representative views of the proposal is provided.⁵² Again, these locations are consistent with those suggested by Auckland Council's landscape specialist and include a viewpoint from Maungawhau, Mt Eden to the south. As well as being supported by the visual simulations, 3D visualisation renders are also provided within the *WAM design documents*, and also the Visual Study document (**Appendix D**) which illustrates consented buildings within the city centre (which are yet to be constructed).

104 Also, dependent on the location of the viewpoint, and therefore the angle of the view toward the proposal, the towers take on varied form and appearance. This is in relation to the chamfered edges, the relationship between the towers and podium, and the treatment of the respective façades. This creates visually interesting buildings in the cityscape and on the skyline. This is discussed further below.

*St Patricks Square*⁵³

105 This viewpoint provides a representative view from St Patrick's Square where a portion of the P1 podium building and the eastern part of the lower levels of T1 will be visible⁵⁴, seen along Federal Street (to the north).

106 The immediate streetscape environment is the main focus of the view from this location, however Tower T1 will also be a focal point (approximately 210m to the north) and will draw the viewers eye up, and along toward the Federal Street and Fanshawe Street intersection.

107 The patterning and material treatment on the southern façades of both P1 and T1 will be evident, showing complementary, yet contrasting buildings that extend the eye upwards. The form and more horizontal nature of the podium

⁵² Taking into account the existing and emerging / consented buildings (including the *WAM – Visual Study* as **Appendix D** to this report).

⁵³ Refer **Figures 2 and 3** within **Appendix B**.

⁵⁴ This is illustrated on the visual simulation (**Figure 3**). To see the upper levels of the towers, the viewer would need to sharply look up.

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will help ground the buildings and provide a relationship to the streetscape below.

- 108 From this location, the existing view toward the site includes an existing established Sheoak tree (*Casurina* sp.) (between Customs Street West and Fanshawe Street), the DTC building and the rear of the M Social building which sits on the northern (farthest) side of the subject site. Only a glimpse view of the harbour is afforded, through the tree and between the HSBC Tower and M Social buildings. There is also a shaft view of the sky and openness beyond these buildings (which implies the harbour) which contributes in a small way to the connection between the central city and harbour. The introduction of T1 to this view will close down that viewshaft to a narrow sliver. Although the view was already impeded, this will result in a further visual disconnection between the harbour and the central city and will affect the legibility of the south to north laneway circulation along Federal Street. However, a small visual connection is still retained.
- 109 As mentioned, the main focus of the view in this area will be at street level, within the public open space, or along Federal Street. Given the scale of the respective towers, and although the eye will be drawn up the proposed buildings, only the lower levels will be 'naturally' seen, e.g. a limited field of view and one would need to draw their angle of view up to see the upper levels. The towers will be viewed in the context of the existing buildings which form part of the immediate context and will positively contribute to the skyline and cityscape.



Viewpoint 1 (thumbnail) Isthmus. *St Patrick's Square / Federal Street*
(Refer to Appendix B)

*Queens Wharf*⁵⁵

- 110 The view from Queens Wharf is representative of people who visit this public space and those who use the Ferry Basin gates (part of the ferry terminal) along the wharf. It is approximately 450m to the northeast of the site.

⁵⁵ Refer **Figures 4 and 5** within **Appendix B**.

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- 111 From this location the upper levels of both T1 and T2 will be visible above the ferry terminal infrastructure and the M Social building. From the southern third of Queens Wharf, T1 will rise behind the HSBC building. From this direction, the chamfer on the upper levels of T1 will be front on, while that on the upper levels of T2 will be in silhouette.
- 112 The towers will be seen in the context of a cluster of existing taller buildings on this western side of the city centre which includes the HSBC tower, the Deloitte Centre building and the PwC Commercial Bay tower proximate to Quay Street, with the Sky Tower forming the backdrop set upon the Federal Street ridge. The form of the Ferry Basin infrastructure, the 'Cloud' and activity within the harbour form the dominant elements in the foreground.
- 113 From this location the towers will be the tallest buildings but will integrate into the skyline and cityscape, which includes buildings of varying heights and forms. The towers will 'bookend' the western end of Quay Street with the Sky Tower remaining the central landmark, left of the towers.
- 114 There is clear separation between the two towers and their slender form is obvious from this location. This is accentuated by the façade treatment and the verticality of the glazing elements. The chamfered form of the corners on the upper levels is also evident which adds to their slender nature.
- 115 From this location, there is a step in the heights of existing buildings toward the north, such as from the PwC Commercial Bay tower toward the Deloitte Centre and HSBC tower. The proposal will be consistent with this pattern with a clear step and transition in height between the towers toward the harbour edge to the north.



Viewpoint 2 (thumbnail) Isthmus. Queens Wharf
(Refer to Appendix B)

Quay Street⁵⁶

- 116 The viewpoint location on Quay Street is approximately 750m to the east of the site. It is representative of people travelling along Quay Street from the east into the central city.

⁵⁶ Refer **Figures 6 and 7** within **Appendix B**.

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- 117 From this location the main focus of the view is the immediate streetscape environment. Views are channelled along Quay Street and are framed by the existing buildings (south / left of view) and alignment of Pohutukawa street trees (north / right). The upper levels of proposed towers T1 and T2 will be visible at the 'back' of the view beyond the PwC Commercial Bay tower and HSBC tower which form intervening elements.
- 118 The chamfered form of the upper level of T1 can be seen in the 'front' (eastern side) of the building from this location, as can the varied treatment of the façades, and the glazed vertical extension of the roof form which creates interest to the buildings and skyline.
- 119 When viewing the city from this location there is an existing transition in building height down toward the harbour edge to the north. This is most evident from the PwC Commercial Bay tower to the HSBC tower and Deloitte Centre. As shown on Viewpoint 3 the transition between the two proposed towers is evident, stepping down to the HSBC tower, the Deloitte Centre and the harbour edge beyond.
- 120 Although the buildings do not immediately front Quay Street, they are located within the waterfront block which fronts this street (between Customs Street and Quay Street). The height and form of the proposed towers is consistent with the AUP provision seeking taller buildings which reinforce the Quay Street east-west connection, integrating well into the visual profile of the skyline and they will not take away from the intention of the HEHCP.



Viewpoint 3 (thumbnail) Isthmus. Quay Street
(Refer to Appendix B)

Sky Tower

- 121 Although the Sky Tower is a privately owned operation views from its viewing platforms and observation decks are accessible to the public and have been assessed as a viewpoint as part of this assessment report.
- 122 The site is located approximately 520m to the north of the Sky Tower. The proposed towers will form part of the midground views from the main observation deck, set amongst other buildings located in downtown Auckland.

- 123 Towers 1 and 2 are proposed to be RL227.2m and 161.9m respectively, some 14.3m above and 51m below the height of the main observation deck⁵⁷, respectively. Although higher than the main observation deck, T1 sits well below the top of Sky Tower's spire (at 328m high above ground level⁵⁸) and its upper viewing decks.
- 124 From the Sky Tower the towers will be obvious elements when looking north with the chamfered upper southwestern edge of T1, the variety of materials proposed (and their vertical arrangement) and the nature of the glazed elements evident. The towers will read as integrated, complementary buildings and will add interest to the view. There is also clear separation between the towers.
- 125 The towers will interrupt views to the waterfront, the Waitematā Harbour, Stanley Point and to a lesser extent Bayswater, and the wider Hauraki Gulf. However, Tower T2 will sit well below the 'landform' of the North Shore. When viewed in this context, other towers such as the PwC Commercial Bay tower (and other consented building projects aligned to the Fanshawe ridgeline and yet to be constructed) also interrupt views of this part of the city and harbour.
- 126 Notably, when viewing from this location the proposed towers are slender in their form and occupy only a small portion of the view. Tower T1 will not interrupt the view out to Rangitoto, the Rangitoto Channel, the balance of the Devonport peninsula (to the east), and the harbour entrance. From the Sky Tower the proposed towers are located to the west of the landmarks of Rangitoto, Takurunga (Mt Victoria) and Maungauika (North Head). The towers will interrupt less of the views from the 360° restaurant and the upper-level viewing deck ('Skydeck') which are higher.
- 127 The view from this location is north along the Federal Street ridge between the Sky Tower and the site of the proposed development where the AUP 'Special Height Area' envisages the city's tallest buildings (and where there are a number of consented buildings⁵⁹). It is therefore reasonable to anticipate other tall buildings in the future between the Sky Tower and the site. The design of the proposed development is consistent with AUP policy *H8.3.(30)(d)(ii)* which encourages '*...well-designed, slender towers on sites identified within the special height area*'. The proposal is consistent with this policy.

⁵⁷ The main observation deck is at RL212.9m.

⁵⁸ Ground level at the Sky Tower is approximately 227m, so the top of the spire is approximately RL355m.

⁵⁹ Refer **Image I**, above.



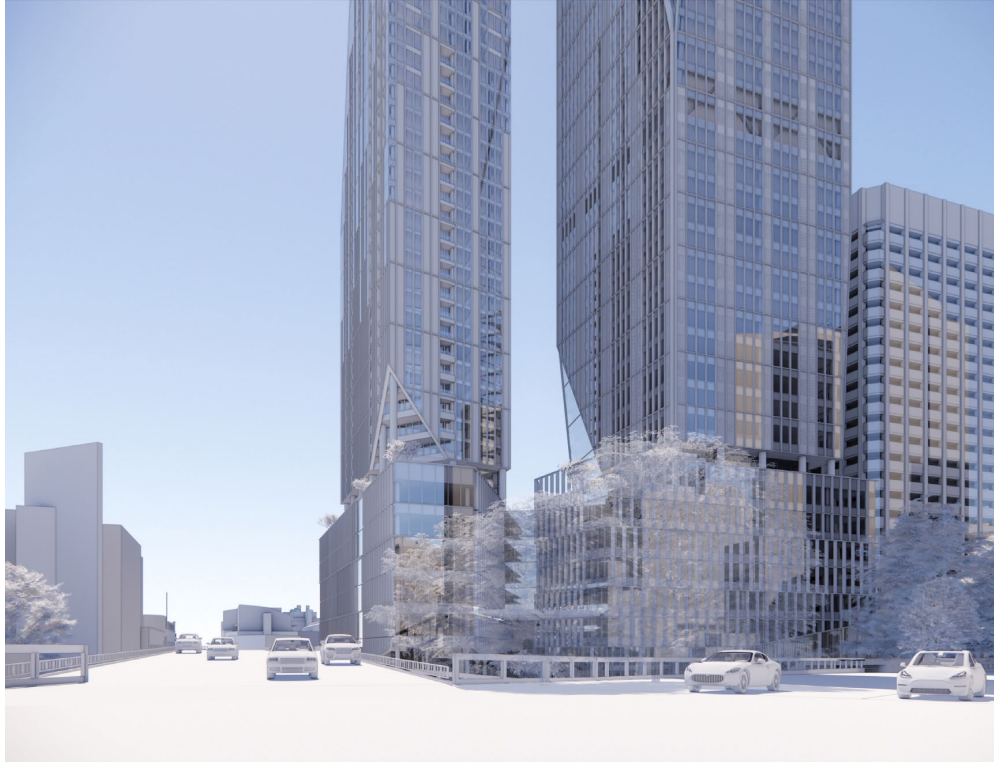
Image Q: Render of the proposal from the equivalent of the main observation deck of Sky Tower.
(Sourced from the WAM Document A – RC80-0005 Rev D).

Hobson Street / Fanshawe Street Intersection⁶⁰

- 128 This viewpoint is located approximately 110m to the southwest of the site at the intersection of Fanshawe Street with Lower Hobson Street. It represents close street views of the proposal from the elevated section of Fanshawe Street across the Lower Hobson Street flyover.
- 129 From this location views are afforded of the southern façades of both towers T1 and T2 and to a lesser extent the western façades of these buildings. The respective towers will be seen set upon the lower podiums which front Lower Hobson Street and Customs Street West. Existing established trees at the junction of Sturdee Street and Customs Street West provide partial screening of these lower levels.
- 130 The chamfered / chiselled form of the lower parts of the respective towers will be evident, as will the vertical glazing treatment which adds visual interest. The view is of the slenderer southern tower façades and there are clear views and visual permeability provided between the buildings.
- 131 Although a reduced setback on the upper levels, there is still an obvious and defined differentiation between the podium and towers. This is successfully achieved through the physical form of the building (including chamfers) and the material and façade treatment of these respective elements.

⁶⁰ Refer to **Image R** below and the 'RFI 58(iiv) Requested Viewpoint – Corner of Lower Hobson St & Fanshawe St looking NE' white card image prepared by WAM which form page 70 of the updated WAM Document **D**.

- 132 Similar to other close locations, the focus of views will be on the streetscape environment but from this location will include the podium and lower levels of the towers. There is only a limited 'natural' field of view afforded and one would need to draw their angle of view up to see the upper levels. The towers will be viewed in the context of existing buildings in the area and will positively contribute to the city and skyline.



*Image R: Viewpoint from Hobson Street / Fanshawe Street intersection (thumbnail) WAM.
(Refer to page 14 of WAM Appendix D)*

*The Viaduct Esplanade*⁶¹

- 133 The view from the Viaduct Esplanade is close at approximately 210m to the west of the site and represents views of users of the Viaduct harbour. The view is framed by existing development (south, right of image) which forms part of the Viaduct and vegetation in the foreground, and the harbour to the north (left). This makes up the main focus of the view from this location.
- 134 In this context the proposal will be viewed above existing buildings and development in the midground and in the context of the HSBC Tower and PwC Commercial Bay tower. From this location the view of the proposal is of Tower T2, which sits in front (to the west) of T1.
- 135 The existing transition in building height toward the harbour edge (as anticipated by the HEHCP) is more difficult to ascertain from this location and is largely limited to the step between the aforementioned buildings and to the

⁶¹ Refer **Figures 8 and 9** within **Appendix B**.

M Social building which illustrates a clear step down toward the harbour. The proposed towers will be seen in this context and when the viewer was to look up there will be a transition in height down to the HSBC and M Social buildings which can be seen in the visual simulation⁶² (refer **Figure 9 – Viewpoint 4** within **Appendix B**). Views along Quay Street⁶³ cannot be afforded due to the angle of the view.

- 136 Given the proximity of the view and the scale of the respective towers, the main focus of the view in this area is within the Viaduct harbour at ground / sea level. Similar to the view from St Patrick’s Square, although the eye will be drawn up to the proposed buildings, only the lower levels will be ‘naturally’ seen, e.g. one would need to draw their angle of view up to see the upper levels. When traveling east through the Viaduct (approaching the site) Tower T2 will be visible above, but will be viewed in the context of, the existing buildings which form part of the immediate context.⁶⁴ Although the proposal will be visible, the main focus of the view when travelling through the area will remain at ground level and the activity within the Viaduct harbour. The towers will form secondary elements where, when one is to look up, T1 will be seen behind T2 which gives perspective depth and will illustrate the transition in height between the forms toward the harbour edge.
- 137 Although viewing the western façade of T2 directly from this location, the patterning, form and material treatment on the façade (including the arrangement of the glazing elements and chamfered edge of the lowest level) will be evident, showing an interesting building that extends the eye upwards and emphasising the tower’s verticality above the podium P2. The roof terrace proposed on top of P2 can be partially seen protruding above the existing residential apartments within the Viaduct harbour development. This helps break up the form of the development overall, differentiating between the podium and tower, and providing a horizontal element upon the podium’s roof terrace.

⁶² Note, the upper levels of the HSBC tower and proposed towers T1 and T2 extend off the top of the simulation page.

⁶³ To equate to the assessment of the Quay Street alignment with taller buildings frontages.

⁶⁴ Refer to the ‘RFI 82 Viaduct Esplanade Sequence’ *white card* images prepared by WAM which illustrate views as one travels west to east through the Viaduct harbour and the extent of the proposal which is visible – see pages 71 – 74 of the updated WAM **Document D**.



Viewpoint 4 (thumbnail) Isthmus. *Viaduct Esplanade*
(Refer to Appendix B)

*Karanga Plaza Steps*⁶⁵

- 138 The view from this location is also reasonably close at approximately 480m to the west of the site. It is a popular tourist location and common for where photographs of the Auckland skyline are captured across the Viaduct Basin.
- 139 From this location the proposed towers will be viewed in the context of the existing pattern and arrangement of buildings on Auckland's skyline which transitions to the harbour edge from the Sky Tower aligned to the Federal Street ridge. This is further emphasised by the consented buildings which span along this ridge.⁶⁶
- 140 The site is located in an area which has a visual 'collective' of taller buildings located along or proximate to Quay Street. These include the HSBC Tower and PwC Commercial Bay tower. The proposal will complement and integrate with this pattern of larger buildings, providing reinforcement of the requirement of taller buildings along the Quay Street east-west frontage and, again, a transition in height between the two towers and the harbour edge.
- 141 When viewed from this location there is a clear existing transition in height to the harbour edge. This is most clearly illustrated by step from the PwC Commercial Bay tower to the HSBC tower and Deloitte Centre buildings (which front Quay Street). The transition in height is retained by the proposal which steps from T1 to T2 down to these buildings and the harbour edge. The angle of the chamfer on the northwestern corner of T2 also assists with emphasising the transition.
- 142 The proposed towers will define this part of downtown, seen as part of the wider visual profile of the skyline. The Sky Tower will be a dominant landmark higher on the ridge at RL355m. The proposed towers will be the prominent landmarks aligned to the ridgeline at the lower end of the city, proximate to

⁶⁵ Refer **Figures 10 and 11** within **Appendix B**.

⁶⁶ Refer **Viewpoint 21 'Proposed Scheme'** within **Appendix D** to this report (WAM Visual Study) which illustrates the consented building at 5-15 Albert Street.

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the harbour edge (on reclaimed land). Also, the 'ridge-valley' form of the skyline is not evident from this location given its side on view from the west.

- 143 The towers will be tall but will be relatively slender⁶⁷ from this angle. Their chamfered form and the visual interest to their façades is evident, as is the vegetation on the roof terrace upon P2. This provides a horizontal element and differentiation between the podium and towers. The proposal presents attractive buildings and will contribute well to Auckland's skyline.



*Viewpoint 5 (thumbnail) Isthmus. Karanga Plaza Steps
(Refer to Appendix B)*

Brigham Street / Hamer Street (Wynyard Point)⁶⁸

- 144 The view from Brigham Street / Hamer Street on Wynyard Point is approximately 800m to the northwest of the site. This view is back toward the central city across Auckland's Viaduct harbour. The viewpoint is on the former 'tank farm' site and landform which protrudes north into the harbour. It is set for future development as a public open space so will become a more popular visitor location in time.
- 145 Similar to the view from Karanga Plaza, from this location the proposed towers will be viewed in the context of the existing pattern of tall buildings on Auckland's skyline which transition from the harbour edge up toward the Sky Tower (including those consented buildings⁶⁹). The existing transition is evident in the photo on **figure 12 (Appendix B)** with the step from the Deloitte Centre building and HSBC tower up to the PwC Commercial Bay tower.
- 146 Again, from this location, the site is located in an area which has a visual 'collective' of taller buildings located along or proximate to Quay Street. The

⁶⁷ As outlined within paragraph 98(d) the slenderness of the western façades of the proposal relates to the how the eastern and western façades of the respective towers, although wider than the northern and southern façades, also have slenderness in terms of the overall building form. This is due to the height and proportions of the respective buildings and these façades, their chamfered / chiselled form and the façade treatment which includes the vertical glazing elements that expresses their verticality and provides depth and shadow, breaking up any apparent massing.

⁶⁸ Refer **Figures 12 and 13** within **Appendix B**.

⁶⁹ Refer **Viewpoint 7 'Proposed Scheme'** within **Appendix D** to this report (WAM Visual Study).

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proposal will complement and integrate with this pattern, presenting taller buildings toward Quay Street and providing a 'bookend' to the western corner of the city.

- 147 From this angle the chamfered corners and façades are evident with a slender form to the view of T1. This adds interest to the towers and will contribute well to the city's visual profile and skyline. Their stepped form will also be obvious providing the transition to Quay Street and the harbour edge consistent with the existing form and building heights, and the purpose of the HEHCP.



*Viewpoint 6 (thumbnail) Isthmus. Brigham Street / Hamer Street (Wynyard Point)
(Refer to Appendix B)*

*Stanley Point*⁷⁰

- 148 The Cyril Bassett VC Lookout is a key viewpoint because it is one of the only public viewpoints of the central city from Stanley Point and it represents the closest view across the water from the north. From this location the 'ridge-valley' pattern of the Auckland skyline is evident. The proposed towers will be consistent with that pattern – adding to the cluster of towers west of Queen Street and aligned with the Federal Street ridge. A number of the consented buildings⁷¹ will also form part of this cluster. The proposed towers will integrate well with these buildings and the cityscape.
- 149 Similar to other viewpoints, the proposed towers will define and 'bookend' the western edge of the city providing buildings of scale. The step down from T1 to T2 and the space between them, which is obvious from this viewing angle, is consistent with the criterion to provide permeability and a transition from the core CBD to the waterfront (in relation to the HEHCP). The chamfered upper levels of both towers is evident, with a strong chiselled, slender silhouette of T2 visible.
- 150 The proposal will not be viewed out of context from this location and presents a considered and integrated design response for this part of the city and also the city's skyline.

⁷⁰ Refer **Figures 14** and **15** within **Appendix B**.

⁷¹ Refer **Viewpoint 11 'Proposed Scheme'** within **Appendix D** to this report (WAM Visual Study).



Viewpoint 7 (thumbnail) Isthmus. Stanley Point
(Refer to Appendix B)

*Takurunga / Mt Victoria, Devonport*⁷²

- 151 This location represents a view from Takurunga / Mt Victoria in Devonport on Auckland's North Shore. The view is toward the central city from the north-east upon the maunga which is a popular location due to the views afforded to the wider city, and to the Waitematā harbour (west) and Hauraki Gulf (north). From this location the city is framed by the harbour (foreground), Maungawhau (east) and Harbour Bridge (west). The Waitakere Ranges can be seen in the distance.
- 152 From this location buildings in the central city are seen in more of a cluster as opposed to the 'ridge-valley' pattern which was more evident from directly to the north from Stanley Point. The Sky Tower forms the tallest building in the cityscape upon the ridgeline. There are also a number of tall, consented buildings⁷³ within the city centre which would also be visible from this location. The transition in building height toward the harbour edge and the alignment of buildings along Quay Street is not immediately obvious from this location. However, there is a step from the PwC Commercial Bay tower to the HSBC tower.
- 153 The proposed towers will appear on the western edge of the cluster and will provide a clear step and transition in height to the harbour. This is further emphasised by the proposed tower being viewed behind the HSBC tower. From this angle the verticality of the materials proposed on the façades and the chamfered form of the respective towers is evident, front on to T1 and a silhouette to T2. This provides visual interest to the skyline.

⁷² Refer **Figures 16 and 17** within **Appendix B**.

⁷³ Refer **Viewpoint 13 'Proposed Scheme'** within **Appendix D** to this report (WAM Visual Study) provides a view from Devonport from a similar, representative viewpoint to Isthmus **Viewpoint 7**.



Viewpoint 8 (thumbnail) Isthmus. Takuranga / Mt Victoria Devonport
(Refer to Appendix B)

*Ōkahu Bay Wharf (Ōrākei)*⁷⁴

- 154 This viewpoint represents a view toward the city across the harbour from the end of the Ōkahu Bay Wharf, in Ōrākei. The viewpoint location is approximately 4.5km to the east of the site. Given the east to west (side on) angle of the view the ‘ridge-valley’ pattern of the skyline is not evident.
- 155 The city’s skyline rises from north to south (right to left) along the alignment and up the respective ridgelines toward the Sky Tower, which forms the highest building on the skyline. Viewpoint 14 of **Appendix D**⁷⁵ illustrates the existing and consented buildings⁷⁶ within the city centre. The consented buildings will provide additional tall buildings to the city’s skyline and the proposal will not be seen as being out of context in relation to location and scale.
- 156 The transition in height from the harbour edge is evident from this location between the HSBC tower and the PwC Commercial Bay tower. The proposed towers will add new elements into this view, proximate to the harbour edge, set behind existing buildings such as the PwC and HSBC towers (at the ‘back of the city’).
- 157 From this angle, again, the chamfered form and the variety and vertical façade treatment of the upper reaches of the towers will be evident. The chamfer on T1 is front to the view and that of T2 forms a silhouette. The stepping and transition down in height from T1 to T2 toward the harbour is clear from this location. The towers will also help visually reinforce the east-west connection and alignment of tall buildings along the Quay Street frontage, providing the ‘bookend’ to the western part of the city.

⁷⁴ Refer **Figures 18** and **19** within **Appendix B**.

⁷⁵ Refer **Viewpoint 14 ‘Proposed Scheme’** within **Appendix D** to this report (WAM Visual Study).

⁷⁶ Yet to be constructed.



Viewpoint 9 (thumbnail) Isthmus. Ōkahu Bay Wharf (Ōrākei)
(Refer to Appendix B)

*Tamaki Drive at The Strand*⁷⁷

- 158 This location provides a view approximately 1.7km to the east of the site representative of people looking toward the city from Tamaki Drive at the junction with the Strand. From this location the city appears as a single clustered arrangement of buildings which step north to south up toward the Sky Tower. The transition of height from the harbour edge can be seen stepping from the Deloitte Centre, up to the HSBC tower and the PwC Commercial Bay tower. There are also taller buildings further to the south within the city including those which are consented.
- 159 The dominant elements in this view include the immediate streetscape environment, the railway and the operations within the Ports of Auckland (north, right of image). The buildings within the central city form the background view.
- 160 The proposed towers will be viewed as the furthest away of the tall buildings from this location with numerous other buildings forming part of the intervening landscape (again, seen at the 'back of the city'). These buildings include PwC Commercial Bay tower and HSBC tower. Although proposed towers T1 will be taller, those in the intervening landscape and closer to the viewpoint appear either taller or equivalent in height due to visual perspective. In particular this includes the Seascapes building (under construction⁷⁸) and the Pacifica building. This will assist the proposed towers to visually integrate into the cityscape.
- 161 The chamfer on the upper level of T1 and the vertical elements on its eastern façade will be evident from this location, providing visual interest.
- 162 This east-west view along Tamaki Drive follows the alignment of Quay Street. Although the towers are setback from the immediate frontage to Quay Street, their scale and form complements the intention of the east-west alignment of

⁷⁷ Refer **Figures 20** and **21** within **Appendix B**.

⁷⁸ At the time of writing.

Isthmus.

taller building along this edge. The step in height between the buildings and the transition to the harbour edge is evident from this location. It is consistent with the existing form and there is a clear transition from T1 down to T2 and then to the HSBC tower and Deloitte Centre toward Quay Street and the harbour edge.



Viewpoint 10 (thumbnail) Isthmus. Tamaki Drive at The Strand
(Refer to Appendix B)

*Maungawhau, Mt Eden*⁷⁹

- 163 The view from the Maungawhau tihi (summit) is approximately 3.6km away and toward the southern side of the central city. Maungawhau is a popular tourist destination and offers extensive, panoramic views around the city to both harbours, the Waitakere Ranges and to other maunga.
- 164 From this location the central city will be viewed as a single cluster of buildings with the Sky Tower framing the city's western built edge. The view is from the southeast and the 'ridge-valley' pattern is marginally apparent.
- 165 Only the upper reaches of T1 will be visible from this location, with T2 screened by existing buildings. Although it will appear as one of the taller buildings in the city, it will be an integrated element as part of the skyline (including the consented buildings which align to the Federal Street ridgeline proximate to the site and Sky Tower⁸⁰). The chamfered edges (particularly on its southwestern corner) emphasise its slender form creating interest to the building and will add a distinctive element to the skyline.
- 166 The roofline of T1 will sit largely aligned with the height of the Sky Tower's 'ring'. T1 will not appear dominant or out of place with the Sky Tower remaining as the most prominent element on the skyline.
- 167 There will not be any additional effect on views toward key natural elements such as the Waitematā harbour, Rangitoto Island, the Rangitoto Channel, and

⁷⁹ Refer **Figures 22** and **23** within **Appendix B**.

⁸⁰ Refer **Viewpoint 15 'Proposed Scheme'** within **Appendix D** to this report (WAM Visual Study).

any other volcanic cones or maunga. The views to the harbour are already compromised from this location by existing buildings in the city.



Viewpoint 11 (thumbnail) Isthmus. *Maungawhau (Mt Eden)*
(Refer to Appendix B)

*Anglesea Street / Ponsonby Road*⁸¹

168 This location represents a view from Ponsonby Road at the intersection with Anglesea Street, approximately 2km to the southwest. The view is channelled down the street toward the central city, flanked by street trees and houses.

169 From this location the central city again appears as a cluster of tall buildings. The Sky Tower forms a central component of the skyline (with the Auckland Council Building and other larger buildings to the south obscured from view by the trees). A number of the tall, consented buildings⁸² are located between the Sky Tower and proposal, aligned to the Federal Street ridgeline. The proposal would be seen in this context.

170 The transition in height toward the harbour edge is evident with the step between the PwC Commercial Bay and HSBC towers. The proposed towers will be prominent in this view, providing tall buildings at the northwestern edge of the city, however the Sky Tower remains the dominant element.

171 The chamfered form of the respective towers is evident, being viewed front on for T1 and as a silhouette for T2. The subtlety of the arrangement of materials proposed across the respective building façades is also evident. These design features help emphasise the verticality and form of the towers and will contribute well to the city skyline. The step in height from T1 down to T2 is evident and provides a clear transition toward the harbour edge. This is further emphasised by the chamfered edge of the upper part of T2 when viewed from this location. The separation gap between the towers is also evident.

⁸¹ Refer **Figures 24** and **25** within **Appendix B**.

⁸² These are illustrated on **Viewpoint 6 'Proposed Scheme'** within **Appendix D** to this report (WAM Visual Study). It is acknowledged that viewpoint 6 is from Franklin Road not Anglesea Street (as per visual simulation viewpoint 12 - **Appendix B**). However, the purpose is representative to show the consented buildings which span between the Sky Tower and the proposal when viewing from the southwest.



Viewpoint 12 (thumbnail) Isthmus. Angelsea Street / Ponsonby Road
(Refer to Appendix B)

*Shelly Beach Road overbridge*⁸³

- 172 This viewpoint represents ‘side angle’ views of the central city skyline from the motorway overbridge (offramp), approximately 2km from the west of the site. Viewers from this location are those using the offramp when approaching the city from the North Shore or walking / cycling across this bridge.
- 173 The dominant components of the view are the motorway and Westhaven marina, with the city’s skyline forming the backdrop.
- 174 The ‘ridge-valley’ form of the city’s building pattern is not evident from this location due to the orientation of the view. However, the broad transition in building height aligned to the Federal Street ridgeline is evident between the harbour edge and the Sky Tower (which forms the tallest element on the skyline to the south). The transition in height of existing buildings to the harbour edge is evident from the PwC Commercial Bay tower, HSBC tower down to the Deloitte Centre and other smaller buildings aligned to Quay Street.
- 175 The stepping down between T1 and T2 is evident from this angle and provides an obvious transition of scale and height to the harbour edge. It is complementary to the existing transition and, as can be seen on the visual simulation, T1 steps down to T2 and then to the HSBC building and Deloitte Centre.
- 176 The chamfered form on T1 will be viewed front on from this location with that of T2 forming a silhouette, providing visually interesting buildings. The design and form of the respective towers are complementary, and they will not be viewed out of scale or context as part of the city’s skyline. It is within the central city’s clustered arrangement where buildings of the greatest scale are anticipated. The materials on the respective façades add interest to the buildings and they will positively contribute to the city skyline.

⁸³ Refer **Figures 26** and **27** within **Appendix B**.

- 177 Again, although the towers are setback from the immediate frontage to Quay Street, their scale and form complements the intention of the east-west alignment of taller building along this edge. The proposed development will be a landmark aligned with the Federal Street ridgeline.



Viewpoint 13 (thumbnail) Isthmus. *Shelley Beach Road overbridge*
(Refer to Appendix B)

*St Mary's Bay Beach*⁸⁴

- 178 This viewpoint also represents 'side angle' views of the central city skyline, in this instance from sea level at St Mary's Bay Beach. It is approximately 1.6km west of the site. Viewers from this location are predominantly those using the bay for recreational purposes such as water sports, walking and cycling along the Westhaven boardwalk, travelling along Westhaven Drive or the motorway.
- 179 The dominant components of the view are the bay itself, the eastern side of Westhaven marina and Wynyard Quarter (which is constantly changing with emerging development) with the city's skyline forming the backdrop.
- 180 Similar to the views from the Shelley Beach Road overbridge, the 'ridge-valley' form of the city is not evident from this location but the broad transition in building height aligned to the Federal Street ridgeline is evident. This is also shown in relation to the existing and consented buildings illustrated on Viewpoint 4 within **Appendix D**⁸⁵, which will add taller buildings to the city's skyline.
- 181 Again, from this location the existing transition in height to the harbour edge between the PwC Commercial Bay tower and the HSBC building is evident. The proposed towers will complement this existing transition and pattern of development providing an obvious step in height between T1 and T2, and down to the HSBC tower toward the harbour edge. From this location and angle of view, the existing buildings within Wynyard Quarter also complement the height transition.

⁸⁴ Refer **Figures 28** and **29** within **Appendix B**.

⁸⁵ Refer **Viewpoint 4 'Proposed Scheme'** within **Appendix D** to this report (WAM Visual Study).

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- 182 The chamfered form on T1 will be viewed front on from this location with that of T2 forming a silhouette, providing visually interesting buildings. The design and form of the respective towers are complementary, and they will not be viewed out of context or scale as part of the city's skyline. The materials on the respective façades add interest to the buildings and they will positively contribute to the city skyline.



Viewpoint 14 (thumbnail) Isthmus. *St Mary's Bay Beach*
(Refer to Appendix B)

*Sulphur Beach Reserve*⁸⁶

- 183 This viewpoint represents a view of the city's skyline across the Waitematā harbour from the motorway (when travelling south). It is from Sulphur Beach Reserve which is located approximately 2.5km to the northwest of the city adjacent to the motorway and harbour bridge.
- 184 From this location the city forms a small part of a wider view. The most dominant elements, aside from the harbour, are the Sky Tower, and the cluster of buildings east on the eastern edge of the city which includes the PwC Commercial Bay tower, the Pacifica building, and the Seascape building (under construction). The consented buildings will also add tall elements to the city's skyline (as illustrated within the **Appendix D**⁸⁷).
- 185 From this location the alignment of buildings spanning east-west along Quay street is difficult to ascertain. The transition in building height back from the harbour edge is also difficult to ascertain, however buildings can be seen located behind the other, such as the HSBC tower can be seen in front of the PwC Commercial Bay tower.
- 186 The proposed towers will provide additional buildings on the skyline, viewed in the context of this emerging and developing cityscape, located proximate to the harbour edge. They will be viewed as two complementary, yet subtly different buildings. They have similar architectural style, form and material composition and read as a pair or 'siblings' in a family. The material treatment

⁸⁶ Refer **Figures 30** and **31** within **Appendix B**.

⁸⁷ Refer **Viewpoint 2 'Proposed Scheme'** within **Appendix D** to this report (WAM Visual Study).

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of the façades is evident which helps express the verticality of the towers. The chamfered form of T1 forms a silhouette and that on T2 is front on in this view.

- 187 The towers will present visually interesting buildings and will contribute recognisable elements to the skyline. The stepping and transition in height to the harbour edge will be evident from this location, with T1 located behind T2, and the difference in height is clear where T2 protrudes further west when viewed from this angle.



Viewpoint 15 (thumbnail) Isthmus. *Sulphur Beach Reserve*
(Refer to Appendix B)

Harbour View Beach Reserve, Te Atatu Peninsula⁸⁸

- 188 This viewpoint represents views from the Te Atatu peninsula, some 9km to the west of the site. From this location the cityscape forms the background to an expansive view which includes the upper Waitematā harbour and extends from the North Shore (north) across the harbour bridge, central city, Pt Chevalier and Mt Albert to Rosebank. Numerous maunga across Auckland's landscape can also be seen.
- 189 From this location the built form of the city appears as a clustered arrangement predominantly to the north of the Sky Tower, which is the most evident and obvious component of the view. The existing transition in building height is apparent.
- 190 From this location the proposed towers will be visible, however they will be viewed as small parts of the central city's built form, which in turn forms a small part of the expansive view of the wider Auckland city context. Although at a great distance, the stepped form and transition in height to the harbour edge between tower T1 and T2 will be evident.

⁸⁸ Refer **Figures 32** and **33** within **Appendix B**.



Viewpoint 16 (thumbnail) Isthmus. Harbour View Beach Reserve, Te Atatu Peninsula
(Refer to Appendix B)

Signage⁸⁹

191 From the broader cityscape, the ‘signage zones’ on the upper levels of the towers will likely be visible (dependent on location and angle of view). Where these ‘zones’ are visible they will be viewed as integrated elements on the respective façades which will not detract from the building’s profile or appearance. The ‘zones’ are of a small scale (relative to the balance of the building) on the upper and middle parts of the northern and southern façades of tower T1 only. Being only two locations of relatively small scale, this avoids any potential adverse visual amenity, dominance or cumulative effects.

Assessment from the broader cityscape summary

192 In summary, the proposed development will provide a significant and positive contribution to the cityscape and skyline of Auckland from locations both near and in the broader landscape. Any potential adverse effects on the wider cityscape have been addressed through the design of the respective buildings and how they sit as part of the city. The towers will define the northern end of the development pattern along Federal Street ridgeline and the western end of the central city. The siting and height of the towers will also integrate positively into the composition and arrangement of buildings in the city.

193 The form of the towers and their façade composition provides visually interesting buildings which have been designed to reflect a strong mana whenua and cultural narrative and read as a pair or ‘siblings’ through their similarities, yet subtle differences.

194 The site (and therefore the towers) is also setback from the harbour edge, with the Viaduct (west) and development fronting Quay Street (north) providing a buffer. Notwithstanding, the proposed towers will provide tall buildings of an appropriate scale which will add to the visual profile of the city and reinforce the east-west connection along Quay Street, and the aforementioned development pattern aligned with the Federal Street ridge.

⁸⁹ Refer **Appendix C** and E23 of the AUP.

195 The development will also continue the 'ridge-valley' pattern in the city (although only truly discernible from views from the north). The slender form, the chamfered edges and the stepped nature between the two towers provide an obvious and sound transition in height toward the waterfront and harbour edge.

Appropriateness and the effects of the proposal in its urban landscape context

196 From a landscape assessment perspective, the site is appropriate for the proposed mixed-use development.

197 The site is strategically located proximate to the waterfront which can provide connection between the central city and harbour edge, including the Viaduct Harbour (and Wynyard Quarter beyond). The proposal will help transform and rejuvenate this part of lower downtown Auckland.

198 The site and these connections are currently hindered by the current drawbacks of the use of the site which includes the poor streetscape amenity, poor activation at the street level, the lack of internal connections and the undistinguished architecture. For these reasons, it is a blighted part of the city. However, the proposed design will resolve these shortcomings and will provide a positive transformation of the site and this part of downtown Auckland. The reasons for this have been discussed earlier in relation to the assessment of the streetscape effects.

199 The proposal will contribute a mix of activities onto a site that has access and proximity to all of the facilities of the central city as well as those of the Viaduct Harbour and Wynyard Quarter. It is also located in the midst of the downtown public transport facilities.

200 For the reasons outlined above in relation to the *broader cityscape*, the proposed design will provide a positive contribution to the Auckland city skyline. The site is an appropriate location for development of this type in relation to the underlying topography, and the city's urban form. The towers will 'bookend' this corner of the city, providing an appropriate transition to the harbour edge whilst reinforcing the alignment of tall buildings along the 'Quay Street east west connection'.⁹⁰

Harbour Edge Height Control Plane (HEHCP)

201 In relation to the infringement of the HEHCP the site is an appropriate location for development of this scale in terms of the underlying topography and urban form of the city. It is aligned with the aforementioned Federal Street ridge, the axis of the Special Height Area from the Sky Tower to Princes Wharf.

⁹⁰ Refer H8.6.5 of the AUP.

202 The proposed towers are also not 'over height' per se (in a zoning sense) because the site is located within the Special Height Area. However, they infringe the localised HEHCP standard which applies to the blocks between Quay Street and Customs Street. The proposal achieves the purpose of the HEHCP through other means as follows:

Transition

- a) The proposal forms a transition between the core central city and the harbour edge because the two towers are set back from Quay Street behind other buildings, with both T1 and T2 located behind M Social. T1 is also inland of the HSBC building.
- b) It also forms a transition from the Viaduct Harbour neighbourhood to the west through the step down in height from T1 to T2, and the separation of Lower Hobson Street and the buildings on the eastern edge of Viaduct Harbour.

Views between the harbour and city centre

- c) The purpose of this provision is achieved as it is to 'maximise' views between the harbour and the city centre. Views between Federal Street and the harbour will be reduced due to the proposal, but not completely lost.

Quay Street east-west connection

- d) While the towers are set back from Quay Street (those frontages are set by the M Social and HSBC buildings), they will nevertheless provide 'tall buildings' to the western end of Quay Street and the north-west corner of the core central city which reinforces the Quay Street east-west connection.

203 In relation to the assessment criteria⁹¹ for exceptions to the HEHCP the following comments are made (acknowledging that the criteria relate to *restricted discretionary* activities whereas the infringement is *discretionary*).

Visual profile.

204 As outlined within the assessment of the *broader cityscape* earlier in this report, the proposed towers will be of a scale, bulk and appearance which represents a visually compatible addition to the city centre.

Waterfront amenity

205 Although the proposed towers penetrate the HEHCP, their bulk will not result in a significant loss of amenity to the adjacent waterfront public areas on the

⁹¹ Refer H8.8.2(8) exception to the harbour edge control plane standard of the AUP.

waterfront (in this case such as Te Wānanga and Queens Wharf on Quay Street) . The towers will be setback and step up from the harbour edge behind the HSBC and M Social buildings beyond Quay Street to the north, and from the existing buildings on the western side of Lower Hobson Street within the Viaduct Harbour.

Streetscape and street corners

- 206 The proposal will provide a revitalised streetscape environment which is at an appropriate scale and form for the site and its context. This is addressed within the *streetscape* assessment section earlier within this report.

Effects on surrounding properties

- 207 This matter is addressed within the urban design report of McIndoe Urban.

Design of upper parts of buildings or structures

- 208 The design avoids an ‘abrupt or arbitrary truncation’ of the upper parts of the building. The form of the proposed towers has been carefully designed including its chamfered / chiselled forms, materiality and roof forms to provide a positive contribution to the Auckland skyline. This is also described within the *broader cityscape* section of this assessment.

Particular constraints

- 209 From a landscape assessment perspective there are no particular site development characteristics which constrained the form of development proposed.

Auckland Plan and City Centre Masterplan

- 210 The proposal helps give effect to the strategy of a ‘quality, compact urban form’ as defined in the Auckland Plan 2050:

- Development occurs in areas that are easily accessible by public transport, walking and cycling;
- Development is within reasonable walking distance of services and facilities including centres, community facilities, employment opportunities, the waterfront and open space; and
- Development maximises efficient use of land.

- 211 The proposal also gives effect to and is consistent with the more concrete moves sought in the City Centre Masterplan 2020:

- Providing quality residential apartments at heart of the central city;
- Enlivening and rejuvenating a run-down corner of the city’s streetscape – helping to enhance the neighbourhood as a residential address;
- Providing a link in the identified laneways network – (sense of place);

- Promoting transit-oriented development, realising the potential of the investment especially of the City Rail Link (CRL);
- Improving the quality of pedestrian connections in the vicinity of Britomart, the waterfront and ferry terminal, and the CRL stations under construction;
- Complementing moves such as the reinforcement of the Quay Street east-west connection, the Waterfront Stitch Streets, the development of this site as identified as part of 'Future Development' under the CCMP, Transit Streets (Fanshawe Street), Hobson and Nelson Street green boulevards, the east-west stitch, and the cycle network;
- Adding to the city's skyline and identity (sense of place);
- Using land efficiently; and
- Reinforcing central Auckland's economic activity which in turn is a significant contributor to the New Zealand GDP.

CONCLUSION

212 The proposal will positively transform and rejuvenate a blighted part of downtown Auckland, proximate to the waterfront and harbour edge. It will replace the existing DTC building with a development which will (i) positively contribute to streetscape, and street level circulation and activity, (ii) positively contribute to the cityscape by introducing a pair of towers with a family likeness to the skyline, and (iii) be an appropriate development in terms of the site's urban landscape context.

213 Although the towers infringe the HEHCP, proposal achieves the purpose by the following means:

- a) Providing a transition between the central city and the harbour edge,
- b) Providing a marker at the western end of Quay Street and reinforcing this east-west connection with tall buildings,
- c) The visual profile of the towers and the design of their 'upper parts' represent a visually compatible addition to the city centre, and
- d) Although reduced, views between the harbour and the central city are retained.

Matthew Jones

Principal

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*Appendix A***METHODOLOGY**

The assessment is consistent with the methodology (the high-level system of concepts, principles, and approaches) of *‘Te Tangi a te Manu: Aotearoa New Zealand Landscape Assessment Guidelines’*, Tuia Pito Ora New Zealand Institute of Landscape Architects, July 2022.

Method

The following summarises the specific method tailored to the landscape context, issues, and purpose of this assessment.

Definition of landscape

The following definition of landscape is adopted:

An area as perceived by people, including how the area is experienced, understood, interpreted, and regarded.

Particularly relevant to this assessment given its urban context is that an **urban landscape** is a type of landscape which falls within the same “*conceptual framework as all other landscapes*”. Furthermore, “...*‘urban landscapes’ do not just mean the natural or green parts of cities. Urban landscapes comprise the physical urban environment (its topography, streets, buildings, open spaces, and their related processes and activities), how people perceive it (its legibility, memorability, aesthetics), and what it means (its identity, history, sense of place)*”.⁹²

Spatial extent of relevant landscape

The relevant context has three nested scales:

- (a) The immediate context is the streetscape of Lower Hobson Street (west), Customs Street West (south), Lower Albert Street (east) and Quay Street (north);
- (b) The mid-scale context is the proposal’s urban context with respect to the core CBD, the waterfront, and Viaduct Harbour. This includes:
 - i. **North.** Along the waterfront, on Princes Wharf and Queens Wharf and to a lesser extent the Port;
 - ii. **East:** Along Quay Street, Customs Streets and Queen Street and the development / land uses along these routes. These areas are zoned *Business – City Centre* and include a variety of different commercial, retail and residential land uses;

⁹² Refer Te Tangi a Te Manu, paragraphs 4.46 – 4.48.

- iii. **South:** Within the city centre along streets (such as Federal Street), existing development and open spaces (such St Patricks Square); and
 - iv. **West:** Along Fanshawe Street, within the Viaduct Harbour, at Wynyard Quarter and on Wynyard Point.
- (c) The wider context is the proposal's place in the city skyline beyond the immediate surroundings, including from Devonport and the North Shore (north), Parnell and Okahu Bay (east), Maungawhau and other maunga (south) and Ponsonby, St Mary's Bay and Te Atatu (west).

Site visits

Site visits were undertaken to gain an appreciation of the site and its wider context. During the site visits, photographs were taken which are illustrated within **Appendix B**.

Proposal

The assessment relies on the AEE Project Description and summarises some of the project's aspects most relevant to assessing landscape and visual effects.

Relevant statutory and non-statutory provisions

Review of statutory planning provisions most relevant to assessing the landscape (including visual) matters (**Appendix C**). The purpose of the review is to frame the landscape assessment, rather than to undertake a planning appraisal.

Existing landscape attributes and values

The assessment describes and interprets the character of the area and evaluates its landscape values. It canvasses relevant physical characteristics, and perceptual and associative qualities. The assessment entailed desk-top review and field work. The report focuses on those matters considered relevant to the assessment of effects.

Issues

The assessment of effects is structured around the following key matters having regard to the nature of the proposal, the relevant provisions, and the urban landscape context.

- a) Potential effects on the **streetscape** (podium scale and appearance, street-level circulation, and activation);
- b) Potential effects on the **broader cityscape** (scale, appearance and context of the towers on the skyline); and
- c) **Appropriateness** and effects of the proposal in its urban landscape context.

Effects

Effects are assessed under each heading. Such effects are assessed against the context of the existing environment and planned urban form, and the outcomes sought in the relevant provisions.

Effects are a consequence on landscape values. Change or visibility themselves are not adverse effects.

The primary focus is on analysing the nature of the effect. Where relevant the nature of effect is also qualified by describing its magnitude using the following 7-point scale against RMA terminology⁹³.



Conclusion

An overall conclusion is made in terms of landscape and visual matters, having regard to all the effects in the context of the nature of the proposal, the landscape values of the locality, and the outcomes sought by the provisions.

Visual simulations

Panoramic photography and the visual simulations have prepared consistent with the *NZILA Best Practice Guide 10.2*. They are provided within **Appendix B** and the method is explained on the last page of that document.

The visual simulations are presented across two A3 pages to provide a field of view of approximately 110°, at correct scale for a 400mm reading distance. Each visual simulation is printed to the same scale for consistency. From some close locations this means that the top of the building is cropped. From those locations architectural renderings are also included as part of the resource consent application documentation to illustrate the building at full height.

The viewpoints were selected to be representative of a range of places in the area and are consistent with those suggested by the Auckland Council landscape specialist.

While visual simulations are useful tools, it is important to note their inherent limitations. Photos are static, have a limited field of view, and tend to flatten perspective. People typically experience landscapes by moving through them in a range of conditions – somewhat differently from photos that are taken in one set of conditions, from fixed viewpoints, and that do not depict wider context.

⁹³ As per *'Te Tangi a te Manu: Aotearoa New Zealand Landscape Assessment Guidelines'*, Tuia Pito Ora New Zealand Institute of Landscape Architects, July 2022.

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The before-and-after format can focus attention on change rather than the effects on landscape / cityscape values. The format discussed above seeks to reduce these inherent limitations.

The visual simulations are also intended to be used in conjunction with other tools including the plan sets and architectural renderings, with the latter being more useful in particular for close street views.

Appendix B

GRAPHIC ATTACHMENTS (*refer separate document*).

Appendix C

RELEVANT STATUTORY PROVISIONS AND OTHER MATTERS

This section outlines the relevant provisions related to landscape assessment matters for the site and underlying zone and precinct as per the Auckland Unitary Plan.

Resource Management Act (1991)

In relation to the RMA, the following sections are relevant:

Section 5(1) - The purpose of this Act is to promote the sustainable management of natural and physical resources,

Section 7(c) - the maintenance and enhancement of amenity values, and

Section 7(f) - maintenance and enhancement of the quality of the environment.

Auckland Unitary Plan – Regional Policy Statement section

Chapter B2 of the AUP is relevant to appraising the landscape and urban design effects of the proposal.

B2 Tāhuhu whakaruruhau ā-taone – Urban growth and form

Objectives and policies are organised under topics. Those relevant to the proposal include:

Quality compact urban form

B2.2.1.(1) A quality **compact urban form** that enables all of the following: (a) a **higher-quality urban environment**; (b) greater productivity and economic growth; (c) better use of existing infrastructure and efficient provision of new infrastructure; (d) improved and more effective public transport; (e) greater **social and cultural vitality**; (f) better maintenance of rural character and rural productivity; and (g) reduced adverse environmental effects.

Quality built environment

B2.3.1.(1) A **quality built environment** where subdivision, use and development do all of the following: (a) respond to the **intrinsic qualities and physical characteristics of the site and area, including its setting**; (b) reinforce the **hierarchy of centres** and corridors; (c) contribute to a diverse mix of **choice** and opportunity for people and communities; (d) maximise resource and **infrastructure efficiency**; (e) are capable of adapting to changing needs; and (f) respond and adapt to the effects of climate change.

- B2.3.2.(1) Manage the **form and design** of subdivision, use and **development** so that it does all of the following: (a) supports the planned future environment, including its shape, landform, outlook, location and relationship to its surroundings, including landscape and heritage; (b) contributes to the safety of the site, street and neighbourhood; (c) develops street networks and block patterns that provide good access and enable a range of travel options; (d) achieves a high level of amenity and safety for pedestrians and cyclists; (e) meets the functional, and operational needs of the intended use; and (f) allows for change and enables innovative design and adaptive re-use.

Auckland Unitary Plan – District Plan section

H8. City Centre Zone

The site falls within the 'H8 Business – City Centre Zone'. The Zone description seeks to ensure the city centre is an international centre for business and learning, innovation, entertainment, culture and urban living. The **greatest** intensity of development in terms of height and floor area is provided for within this zone. The Zone description refers to the contribution of the city centre to Auckland's sense of identity. It notes that *"the **significant height and scale of buildings in the city centre increases their visibility from many places, affecting the quality of both public and private views at local and city-wide scales. In addition to managing the scale of development, the zone manages the **quality of building design** to ensure new buildings successfully integrate with the city centre's **existing and planned built form and public realm** to create an **attractive and recognisable skyline.**"***

The site is located within the **Downtown West Precinct**.

General objectives for all centres, Business – Mixed Use Zone, Business – General Business Zone and Business – Business Park Zone include:

- H.8.2.(1) A strong network of centres that are **attractive environments** and attract ongoing investment, promote commercial activity, and provide employment, housing and goods and services, all at a variety of scales.
- H.8.2.(2) Development is of a **form, scale and design quality** so that centres are reinforced as focal points for the community.
- H.8.2.(3) Development positively contributes towards **planned future form and quality**, creating a **sense of place**.
- H.8.2.(4) Business activity is distributed in locations, and is of a **scale and form**, that:
- (a) Provides for the community's social and economic needs;
 - (b) Improves community access to goods, services, community facilities and opportunities for social interaction; and
 - (c) Manages adverse effects on the environment, including effects on infrastructure and residential amenity.

- H.8.2.(5) A network of centres that provides:
- (a) A framework and context to the functioning of the urban area and its transport network, recognising:
 - (i) The regional role and function of the city centre, metropolitan centres and town centres as commercial, cultural and social focal points for the region, sub-regions and local areas;
 - (ii) Local centres and neighbourhood centres in their role to provide for a range of convenience activities to support and serve as focal points for their local communities;
 - (b) A clear framework within which public and private investment can be prioritised and made; and
 - (c) A basis for regeneration and intensification initiatives.

Business – City Centre zone objectives include:

- H.8.2.(6) The city centre is an internationally **significant centre for business**.
- H.8.2.(7) The city centre is an attractive place to **live, learn, work** and **visit** with 24-hour vibrant and vital business, education, entertainment and retail areas.
- H.8.2.(8) Development in the city centre is managed to **accommodate** growth and the **greatest intensity of development** in Auckland and New Zealand while respecting its valley and ridgeline form and **waterfront setting**.
- H.8.2.(9) The distinctive **built form, identified special character** and functions of particular areas within and adjoining the city centre are maintained and enhanced.
- ...
- H.8.2.(11) The city centre is **accessible** by a range of transport modes with an increasing percentage of residents, visitors, students and workers choosing walking, cycling and public transport.

General policies for all centres, Business – Mixed Use Zone, Business – General Business Zone and Business – Business Park Zone include:

- H.8.3.(1) Reinforce the **function of the city centre**, metropolitan centres and town centres as the primary location for commercial activity, according to their role in the hierarchy of centres.
- H.8.3.(2) **Enable an increase in the density**, diversity and quality of housing in the centres zones and Business – Mixed Use Zone while managing any reverse sensitivity effects including from the higher levels of ambient noise and reduced **privacy** that may result from non-residential activities.
- H.8.3.(3) Require development to be of a **quality** and **design that positively contributes** to:
- (a) Planning and design outcomes identified in this Plan for the relevant zone;

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- (b) The **visual quality** and **interest of streets** and other public open spaces; and
 - (c) **Pedestrian amenity**, movement, safety and convenience for people of all ages and abilities.
- H.8.3.(4) Encourage **universal access** for all development, particularly medium to large scale development.
- H.8.3.(5) Require large-scale development to be of a **design quality** that is commensurate with the **prominence** and **visual effects** of the development.
- H.8.3.(6) Encourage buildings at the **ground floor to be adaptable** to a range of uses to allow activities to change over time.
- H.8.3.(7) Require at grade parking to be located and designed in such a manner as to avoid or mitigate adverse impact on pedestrian amenity and the streetscape.
- ...
- H.8.3.(11) Require development to avoid, remedy or mitigate adverse wind and glare effects on public open spaces, including streets, and **shading** effects on open space zoned land.
- H.8.3.(12) Recognise the functional and operational requirements of activities and development.
- H.8.3.(13) In identified locations within the centres zones, Business – Mixed Use Zone, Business – General Business Zone and Business – Business Park Zone enable **greater building height** than the standard zone height, having regard to whether the greater height:
- (a) Is an **efficient use of land**;
 - (b) Supports **public transport, community infrastructure** and contributes to centre **vitality** and **vibrancy**;
 - (c) Considering the size and depth of the area, can be accommodated without significant adverse effects on adjacent residential zones; and
 - (d) Is supported by the status of the centre in the centres hierarchy, or is adjacent to such a centre.
- H.8.3.(14) In identified locations within the centre zones, Business – Mixed Use Zone, Business – General Business Zone and Business – Business Park Zone, reduce building height below the standard zone height, where the standard zone height would have significant adverse effects on identified special character, identified landscape features, or amenity.

Business – City Centre zone policies include:

- H.8.3.(15) Provide for a wide range and diverse mix of activities that enhance the **vitality, vibrancy** and **amenity** of the city centre including:
- (a) **Commercial** and **residential** activities;

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- (b) Arts, entertainment, events, civic and community functions;
 - (c) High-quality **visitor experiences**, visitor accommodation and associated services; and
 - (d) Learning, teaching and research activities, with a particular concentration in the learning precinct.
- H.8.3.(16) Enable a significant and diverse **residential population** to be established and maintained within a range of living environments and housing sizes.
- H.8.3.(17) Enable the most significant **concentration of office activity** in Auckland to locate in the city centre by providing an environment attractive to office workers, with a focus on the core central business district.
- H.8.3.(18) Provide for a wide **range of retail activities** throughout the city centre while maintaining and enhancing the vitality, vibrancy and amenity of core retail areas within the city centre and centres outside of the city centre. In particular:
- (a) Enable smaller scale retail activities to occur throughout the city centre;
 - (b) Encourage large department stores and integrated retail developments to locate within the core retail area; and
 - (c) Avoid large department stores and integrated retail developments locating outside the core retail area where they would adversely affect the amenity, vitality and viability of core retail areas within the city centre and/or centres outside of the city centre.
- H.8.3.(19) Provide for a **wide range of activities** along the waterfront, while continuing to provide for those activities requiring a harbour location.
- H.8.3.(20) **Enhance the waterfront as a major gateway to the city centre** and Auckland.
- ...
- H.8.3.(22) Support the development of **public transport, pedestrian** and cycle **networks** and the ability to efficiently change transport modes.

Precincts

- H.8.3.(23) Identify and encourage specific outcomes in areas of the city centre that relate to: (a) a distinctive **built character**; and/or (b) a concentration of particular activities; and/or (c) activities that have specific functional requirements; and/or (d) significant **transformational development opportunities**.
- H.8.3.(24) Encourage comprehensive and **integrated development** of key development sites or precincts in the city centre.
- ...

City Form

- H8.3.(29) Enable the **tallest buildings** and the greatest density of development to occur **in the core central business district**.
- H8.3.(30) Manage adverse effects associated with **building height and form** by:
- (a) **Transitioning building height** and development densities down to neighbourhoods adjoining the city centre and to the **harbour edge**;
 - (b) Protecting sunlight to identified public open spaces and view shafts;
 - (c) Requiring the height and form of new buildings to respect the **valley and ridgeline form** of the city centre and building design to be complementary to existing or planned character of precincts; and
 - (d) Managing the **scale, form** and **design** of buildings to:
 - (i) Avoid **adverse dominance** and/or **amenity effects** on streets and public open space; and
 - (ii) Encourage well-designed, **slender towers** on sites identified within the **special height area on Map H8.11.3**.
- H8.3.(31) Maximise **light** and **outlook** around buildings.
- H8.3.(32) Encourage **public amenities** to be provided within developments, including publicly accessible open space, works of art and through site links.

Public Realm

- H8.3.(33) Require building and development of the **highest quality** that contributes to the city centre's role as an international centre for business, learning, innovation, entertainment, culture and urban living.
- H8.3.(34) Require building frontages along identified public open spaces and streets to be designed in a way that provides a sense of **intimacy, character, interest** and **variation**, and **enclosure at street level**.
- H8.3.(35) Require the demolition of buildings and structures to avoid, remedy or mitigate significant adverse effects on the **pedestrian amenity** of the city centre and the safety and efficiency of the road network.
- H8.3.(37) Enable **high-quality public open spaces** along the waterfront that are accessible and provide spaces for recreational opportunities, facilities and events.

Development standards relevant to the assessment include:

- H.8.6.2. **General building height.** Purpose: manage the height of buildings within the city centre to:
- enable the tallest buildings within the core central business district and transition heights down to neighbourhoods adjoining the city centre and to the harbour edge;
 - respect the valley and ridgeline form of the city centre and the existing or planned character of precincts; and

- **avoid adverse dominance, shading and/or visual amenity** effects of building height **on streets** and public open spaces.

H8.6.5. Harbour edge height control plane

Purpose: **manage the scale of buildings** at the western end of Quay Street to:

- provide a **transition in building height** from the core central business district to the waterfront;
- **maximise views** between the harbour and the city centre; and
- reinforce the **Quay Street east west connection** running from the corner of The Strand and Quay Street to the east and Jellicoe Street in Wynyard Precinct to the west by the **alignment of tall building frontages**.

- (1) The height of a building subject to this standard must not exceed the height plane shown in Figure H8.6.6.1 Harbour edge height control plane. The height plane starts at a line 40m above the centre line of Quay Street and continues as a tilted plane at 45 degrees to the horizontal from that line in a southerly direction.
- (2) For the purpose of this standard, the centre line of Quay Street extends between the eastern boundary of Britomart Place and the western boundary of Lower Hobson Street and is defined by a straight line passing through the coordinates specified in Figure H8.6.6.2 Harbour edge height control co-ordinates.

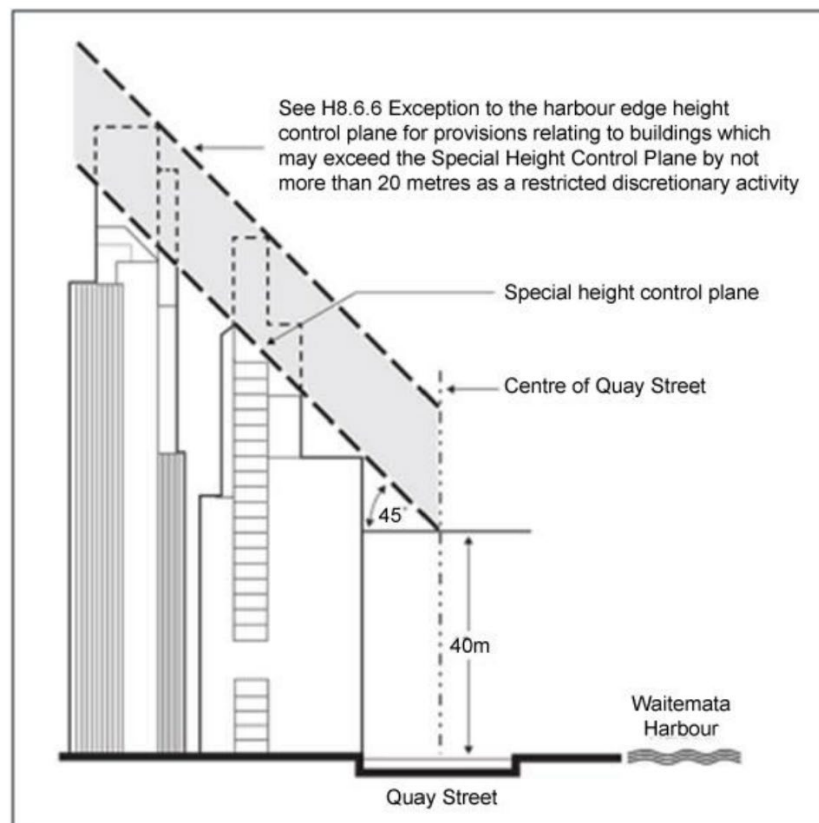
H8.6.6. Exception to the harbour edge height control plane

- (1) Where the building or structure is located on a site within the area bounded by **Customs Street, Lower Hobson Street, Quay Street and Queen Elizabeth Square**, an application may be made as a restricted discretionary activity to exceed the Harbour Edge Height Control Plane by no more than 20m, where the following requirements are met:
 - (a) any **penetration of building bulk** through the Harbour Edge Height Control Plane must be **compensated for by equivalent open space “corridors”** which are situated below the plane and which must:
 - (i) be **continuous and run approximately north to south** through the development site to provide some **permeability of appearance** when looking from a northerly or southerly direction; and
 - (ii) have a **minimum width** equating to 15 per cent of the widest east-west dimension for the site.
 - (b) the method of calculating the compensatory open space for Standard H8.6.6.1(a) above is as follows:
 - (i) establish a maximum total floor area ratio for the site;

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- (ii) calculate and demonstrate the obtainable floor area ratio for the building proposal within the Special Height Control Plane;
- (iii) determine the floor area which the building proposal could qualify for above the Special Height Control Plane;
- (iv) add that floor space to the building or structure above the Special Height Control Plane and also add a theoretical compensatory floor area (equal to that calculated in Standard H8.6.6.1(b)(iii) above) to the building or structure below the Special Height Control Plane, assuming a floor-to-floor distance the same as applies to the building or structure at that level; and
- (v) then recalculate any light and outlook bonus claimed, as if the theoretical compensatory floor area Standard H8.6.6.1(b)(iv) existed.

Figure H8.6.6.1 Harbour edge height control plane



H8.6.9 **Rooftops.** Purpose: ensure the roofs of buildings are **uncluttered** when viewed from the street and surrounding buildings.

- (1) Rooftop projections including towers, turrets, chimneys, lift towers, machinery rooms and water towers that exceed the height of all parts of a parapet surrounding the roof on which the projections are located, must be **enclosed** in a maximum of three structures and **integrated within the overall roof design**.

H8.6.24 **Maximum tower dimension, setback from the street and tower separation.** Purpose: ensure that high-rise buildings:

- are **not overly bulky** and are **slender in appearance**;
- provide adequate sunlight access to streets;
- provide a consistent **human-scaled edge to the street**;
- provide adequate sunlight and outlook around buildings;
- enable views through the city centre; and
- mitigate adverse wind effects.

(2) On every site identified as **special height area** on Map H8.11.3

(a) the maximum plan dimension of that part of the building 28m above mean street level must **not exceed 50m**; and

(b) the part of a building above 28m must be located at least **6m from all boundaries** of the site.

H8.8.1 Matters of Discretion

For **new buildings** (under H8.8.1.(1)) Council restricts its discretion to matters including:

H8.8.1(1) (a) building design and external appearance;

(b) form and design of buildings adjoining historic heritage places;

(c) the design of parking, access and servicing;

(d) the design and layout of visitor accommodation; and

(e) functional requirements.

...

H8.8.1(6) Infringement of building height, building in relation to boundary, streetscape improvement and landscaping, maximum tower dimension and tower separation standards:

(a) effects of **additional building scale** on neighbouring sites, streets and public open spaces (sunlight access, **dominance, visual amenity**, and **landscape character**);

(b) consistency with the **planned future form and character** of the area/zone; and

(c) site specific characteristics;

...

H8.8.1(8) Exception to the **harbour edge height control plane** standard:

(a) form, scale and dominance effects;

(b) amenity effects;

(c) consistency with the streetscape; and

(d) design of buildings or structures;

Relevant Assessment Criteria include:

H8.8.2.(1) new buildings ...:

(a) building **design and external appearance:**

Contributing to a sense of place

- (i) The extent to which the design of buildings **contribute to the local streetscape and sense of place** by responding positively to the existing and planned **form** and **character** of the surrounding area and significant natural landforms and landscape features;
- (ii) The extent to which the **silhouette of the building** as viewed from areas surrounding the city centre positively **contributes to the city centre's skyline;**

Creating a positive frontage

- (iii) The extent to which buildings have **clearly defined public frontages** that **address the street** and public open spaces to positively contribute to the **public realm and pedestrian safety;**
- (iv) Whether the ground floor of a new building is at the same level as the adjoining street;
- (v) The extent to which **pedestrian entrances** are located on the street frontage and are clearly identifiable and level with the adjoining frontage;

...

(viii) where not required by a standard, **activities that engage and activate streets** and public spaces are encouraged at ground and first floor levels;

(ix) the extent to which **internal space** at all levels within buildings is designed to **maximise outlook onto street** and public open spaces;

...

Variation in building form/visual interest

(xii) the extent to which buildings, including alterations and additions, are designed as a **coherent scheme** and demonstrate an **overall design strategy** that **contributes positively to the visual quality** of the development;

...

(xiv) the extent to which buildings are designed to:

- **avoid long, unrelieved frontages** and **excessive bulk** and **scale** when viewed from streets and public open spaces;

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- **visually break up their mass** into distinct elements to reflect a human scale and the typical pattern of development in the area; and
- differentiate ground, middle and upper level;

techniques to achieve this include the use of recesses, variation in building height and roof form, horizontal and vertical rhythms and facade modulation and articulation;

(xv) whether **blank walls are avoided** on all levels of building frontages **to streets** and public open spaces;

(xvi) whether side or rear walls without windows or access points are used as an opportunity to introduce creative architectural solutions that **provide interest** in the facade including **modulation**, relief or surface detailing;

(xvii) the extent to which buildings provide a variety of **architectural detail** at ground and middle levels including maximising doors, windows and balconies overlooking the streets and public open spaces;

(xviii) the extent to which **roof profiles** are designed as part of the overall building form and contribute to the **architectural quality of the skyline** as viewed from **both ground level** and the **surrounding area**. This includes **integrating plant**, exhaust and intake units and other mechanical and electrical equipment into the overall rooftop design;

(xix) the extent to which **colour variation** and **landscaping** are used. Noting they should not be used to mitigate a lack of building articulation or design quality;

(xixa) the extent to which **glazing is provided on street** and public open space frontages and the benefits it provides in terms of (i) the attractiveness and pleasantness of the street and public open space and the amenity for people using or passing through that street or space; (ii) the degree of visibility that it provides between the street and public open space and the building interior; and (iii) the opportunities for passive surveillance of the street and public open space from the ground floor of buildings.

...

H8.8.2.(6) infringement of **building height**, building in relation to boundary, streetscape improvement and landscaping, maximum tower dimension and tower separation standards:

(a) effects of additional building **scale** on neighbouring sites and streets and public open spaces (sunlight access, **dominance**, **visual amenity** and landscape character):

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- (i) whether minor height infringements may be appropriate where it would provide an attractive and integrated roof form that also meets the purpose of the standard;
 - (ii) whether height infringements may be appropriate on corner sites to reinforce the prominence of the corner where it meets the relevant standard infringement criteria and makes a positive contribution to the streetscape; and
 - (iii) whether minor infringements to the building in relation to boundary standard or the streetscape improvement and landscaping standard may be appropriate where the spacious landscaped character of the area is retained.
- (b) consistency with the **planned future form** and **character** of the zone/area
- (c) site specific characteristics:
- (i) whether there are particular site specific characteristics in terms of unusual site size, shape or orientation, or the location and nature of existing buildings which have constrained the form of the development proposed;

H8.8.2.(8) exception to the **harbour edge height control plane** standard:

- (a) visual profile:
- (i) the effects of the **building profile on the form of the city centre** when viewed from the main city centre approaches by land and water and from public places which offer comprehensive views. Whether the building must be of a **scale, bulk, appearance** and **location** which represents a **visually compatible addition** to the city centre.
- (b) waterfront amenity:
- (i) whether the **building bulk** penetrating the Harbour Edge Height Control Plane results in **significant loss of amenity** to adjacent waterfront public areas. Particular consideration will be given to sunlight admission, shading and air movement at street level and at public gathering places.
- (c) streetscape and street corners:
- (i) the extent to which the proposed building is consistent with the **scale, visual harmony** and **form** of the **existing streetscape**, in particular the relationship to street corners, especially for sites fronting Quay Street, Customs Street and Queen Elizabeth Square.
- (d) effects on surrounding properties:

- (i) whether the building allows for adequate **light, space** and **general amenity** around the development and where possible through the site while noting that it is not the intention of this provision to **protect views from private property** to the harbour.
- (e) design of upper parts of buildings or structures:
- (i) the extent to which the part of the building which protrudes through the harbour edge height control plane is designed in a manner which **avoids abrupt or arbitrary truncation** of the upper parts of the building or structure.
- (f) particular constraints:
- (i) whether there are particular site development characteristics in terms of unusual site size, shape or orientation, or the location and nature of existing buildings which have constrained the form of the development proposed.

I205. Downtown West Precinct.

I205.1. Precinct description

“The Downtown West Precinct is located within the heart of the city centre waterfront, between the established Viaduct Harbour Precinct and regenerating Britomart Precinct. The precinct has a mix of commercial and residential land uses including a major hotel and open spaces.

The block bounded by lower Queen Street, Customs Street West, lower Albert Street and Quay Street is generally held in single ownership and has significant redevelopment potential. Queen Elizabeth Square is legally recognized as a road with pedestrian mall status under the Local Government Act 1974. As such, no zone has been applied to the square. As part of any future development proposal, alternative locations for the square should be investigated and identified, potentially including a combination of both publicly provided open spaces, pedestrian and cycle connections and enhanced through site links within the block.

Council has confirmed approval to sell or lease part of Queen Elizabeth Square to ensure that redevelopment of the block is of the highest ‘world class’ design quality and the proceeds from the sale of Queen Elizabeth Square are reinvested in new or enhanced downtown public space/s and collectively, these alternative spaces should achieve a better street and open space network than is presently offered within or immediately adjoining the precinct. If the road stop process is approved, the square can be closed and the precinct provisions together with the Business – City Centre Zone objectives, policies and rules will apply.

The land in the Downtown West Precinct is zoned Business – City Centre Zone and Open Space – Informal Recreation Zone.”

The objectives are:

- I205.2(1) The precinct contains a **mix of uses** and the **form** and **scale** of development is **integrated** with the core central business district and the waterfront.
- I205.2(2) **High quality public open space** is provided and **pedestrian connectivity** from the core central business district to the waterfront and from Britomart to the west is enhanced.
- I205.2(3) Development recognises the precinct's location as part of Auckland's primary transport centre and supports the transport facilities within and around the precinct.

The policies are:

- I205.3(1) Enable and encourage a **diverse range of activities** within the precinct including residential (including visitor accommodation), **commercial**, recreational, temporary activities and events.
- I205.3(2) Encourage an **integrated network of attractive streets, lanes and pedestrian connections** to improve **pedestrian permeability** and accessibility through the precinct and supports the transport interchange function of the area.
- I205.3(3) Provide for an **interconnected network of high quality public open spaces and publicly accessible spaces** which vary in form and function in highly accessible locations within the precinct that are activated by uses around their periphery.

The overlay, Auckland-wide and Business – City Centre Zone objective and policies apply in this precinct in addition to those specified above.

I205.6 Standards

I205.6.2 Pedestrian connections

- (1) Upon the erection of any new building on either of the blocks between Lower Queen Street and Lower Hobson Street, and unless already provided, an **at-grade, east-west pedestrian connection across the block** must be provided, and must achieve the following:
 - (a) provide straight pedestrian routes between streets, with a clear pedestrian movement width of at least 5 metres;
 - (b) protect pedestrians from the weather;
 - (c) incorporate natural daylight through glazed canopies of glazed roof structures;
 - (d) be publicly accessible 24 hours a day, 7 days a week; and
 - (e) incorporate active uses at ground floor level framing the pedestrian connection;

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Purpose: To support the transport interchange function of the area by facilitating **clear** and **unobstructed pedestrian movement** through the precinct at-grade.

Note: The above requirement applies only to the development site, and accordingly it is acknowledged that the laneway may be developed incrementally, particularly across the block bounded by Lower Albert Street, Customs Street West, Lower Hobson Street and Quay Street.

- (2) Redevelopment of the block between Lower Queen Street and Lower Albert Street must include an **at-grade, north-south pedestrian laneway connection** between Customs Street West and Quay Street.

Purpose: To support pedestrian movement between the City Centre Core and the waterfront.

- (3) Redevelopment of the block between Lower Albert Street and Lower Hobson Street must include an **at-grade, north-south pedestrian laneway connection between Customs Street West and Quay Street, being generally aligned with Federal Street.**

Purpose: To support pedestrian movement between the City Centre Core and the waterfront.

I205.8.1 Matters of Discretion

I205.8.1(1) **new buildings**, and alterations and additions to buildings:

- (a) the matters of discretion in H8.8.1(1) for new buildings and external alterations and additions to buildings not otherwise provided for; and
- (b) the **location, bulk** and **scale** of buildings relative to overall development, including the layout and design of roads, pedestrian linkages, open spaces, earthworks areas and land contours, and infrastructure location; and
- (c) **design, bulk** and **location** of buildings;

...

I205.8.1(4) infringing the **pedestrian connections** standard:

- (a) **functional, safe**, and **convenient pedestrian access** between Lower Queen Street and Lower Hobson Street, and between Customs Street West and Quay Street.

Relevant Assessment Criteria include:

I205.8.2(1) new buildings ...:

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- (b) the assessment criteria in H8.8.2(1) of the Business - City Centre zone rules for new buildings and/or alterations and additions to buildings apply.
- (c) the **proposed building**, alteration or addition relative to the location of infrastructure servicing the area and open space should result in an **integrated network** that is adequate to meet the needs of the overall development area;

...

I205.8.2(4) infringing the **pedestrian connections** standard:

- (a) whether development that infringes the pedestrian connection standard demonstrates that:
 - (i) there is a **safe, legible and direct link** through the block;
 - (ii) the width of the lane or link is sufficient to provide a **functional connection** between the adjoining streets;
 - (iii) there is adequate provision for pedestrian movement in **support of existing east-west options**, and in support of the transport interchange function of the area; and
 - (iv) **north-south pedestrian movement options are increased** through blocks in support of pedestrian flows between the City Centre core and the waterfront, with a focus on the quality and interest provided through and along those routes.
- (b) consistency with the **planned future form and character** of the Downtown Precinct:
 - (i) where the pedestrian connection standard is infringed, whether the proposal demonstrates that Policies I205.3(2) and I205.3(3) above are met.

E23 Signs

E23.2. Objectives

...

- (2) Billboards and comprehensive development signage are managed to maintain traffic and pedestrian safety, historic heritage values and the **visual amenity values** of buildings and the surrounding environment.

E23.3. Policies [rcp/dp]

...

- (2) Require the placement, location and size of billboards and comprehensive development signage on buildings to not significantly

detract from the profile or appearance of a building, or cover any significant architectural features on the façade of a building.

- (3) Enable billboards and comprehensive development signage while avoiding signs creating **clutter** or **dominating** the building or environment by controlling the size, number and location of signs.

...

- (5) Manage the effects of billboards and comprehensive development signage to maintain the values of scheduled historic heritage places and **visual amenity values**.

E23.8.2. Assessment criteria

The Council will consider the relevant assessment criteria for restricted discretionary activities in Activity Table E23.4.1 Billboards in zones and Activity Table E23.4.2 Billboards on street furniture in road reserves, existing lawfully established billboards and comprehensive development signage from the list below:

- (1) **visual amenity, scale and location:**
 - (a) the extent to which comprehensive development signage, free-standing billboards, or billboards on a side, rear or street facing building façade are appropriate in terms of the zone they are located in taking into account all of the following:
 - (i) the **scale, form** and type of signs or billboard;
 - (ii) the location of the signs or billboard in relation to other signs and billboards and adjacent structures and buildings;
 - (iii) the size of the site in which the signs or billboard will be located;
 - (iv) the relationship with the **streetscape, landscape** and **open space** areas in the vicinity of the proposed signs or billboard; and
 - (v) the length of time the signs or billboard will be in place.
 - (b) the extent to which the signs or billboard, when attached to a building, will appear to be an **integrated element** of the building and positively relate to structural bays, structural elements, architectural features, building proportions and the overall design of the building;
 - (c) the extent to which the signs or billboard structure **are visually integrated** with the existing built and/or natural environment;
 - (d) whether the signs or billboard structures are **simple** and **visually recessive** in terms of form and colour, and are designed to discourage access for graffiti on the image displayed;

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- (e) whether the signs or billboard detracts from the **visual amenity** of any **public open space** from which it can be seen, including the characteristics of the streetscape, natural environment, landscaping and open space;
- (f) whether the signs or billboard, if located in close proximity to a scheduled historic heritage place, adversely affects the **visual amenity** or detracts from the **visual qualities** that are fundamental to the historic heritage values of the scheduled historic heritage place;
- (g) whether a sign or billboard that will replace an existing sign or billboard on a building integrates with the building's form, shape and architectural features; and
- (h) whether the sign or billboard will **dominate** the outlook from any dwelling or public open space.

...

(4) cumulative effects:

- (a) whether the signs or billboard, in conjunction with existing signs nearby, will create **visual clutter** or other adverse **cumulative** effects.

Other matters (non-statutory documents)

The City Centre Masterplan 2020

The City Centre Masterplan (CCMP) is a guiding document that sets a vision for the city centre over the 20 years. It sets out ten strategic outcomes.

Outcome 1: **Tāmaki Makaurau - Our place in the world.** Our vision is for a city centre that actively recognises and celebrates Auckland's historic heritage as a driver of positive change and placemaking.

Outcome 2: **Connected city centre.** We aim to enable safe, healthy and sustainable travel options to improve people's access and choice of transport modes into and around the city centre.

Outcome 3: **Accessible and inclusive city centre.** We want to achieve a city centre that is inclusive and welcoming to all in Tāmaki Makaurau.

Outcome 4: **Green city centre.** By restoring our biodiversity and ecological systems (Mauri Tu) we will deliver a healthy and happy city centre.

Outcome 5: **Public life. Public space** or realm is the **glue that holds the city centre together.** It is the canvas for public life. It needs to work well for all Aucklanders and visitors to Tāmaki Makaurau.

- Outcome 6: **Residential city centre neighbourhoods.** Auckland's city centre is an increasingly popular place to live. As the city centre population grows and matures, this outcome shapes the city centre's **public realm, housing supply** and **social infrastructure** to deliver a highly **liveable city centre**.
- Outcome 7: **Quality built form.** We want to deliver a well-designed and planned city centre.
- Outcome 8: **Heritage defined city centre.** Our vision is for a city centre that actively recognises and celebrates Auckland's historic heritage as a driver of positive change and place-making.
- Outcome 9: **Sustainable city centre.** Auckland city centre will address the challenges of global climate change and urban growth via urban design. This part of the CCMP is shaped by targets and direction from the Auckland Climate Action Framework.
- Outcome 10: **Prosperous city centre.** We want to set out practical ways to develop the city centre so it can continue to thrive as an economic centre and cater for the needs of our diverse population.

Appraising the proposal against these strategic outcomes includes its contributions to a **residential city centre neighbourhood, quality design, public space, identity, and proximity** to transport, active modes, and city centre activities.

The CCMP also outlines eight transformational moves.

- TM1: **Māori outcomes.** This transformational move anticipates a range of interventions and systemic changes to bring mana whenua presence, Māori identity and life into the city centre and waterfront.
- TM2: **The east and west stitch.** Land at both the east and west edges of the city centre is under-used and under-valued and cut off by major roads. This move proposes to stitch the city centre together.
- TM3: **Waihorotiu/Queen Street Valley.** This move proposes to make the area more accessible, more attractive and more prosperous, with pedestrian-friendly streets and better connections to the rest of the city centre.
- TM4: **The Learning Quarter.** This move will integrate Auckland's city centre universities into city centre life.
- TM5: **Transit oriented development.** This move will shape planning and development in the best-connected areas of Auckland, maximising the benefits of investment in public transport and associated public realm.

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- TM6: **The Green Link.** We want to unite some of our most important and historic parks and open spaces through a network of tee-line linear park spaces along the Victoria Street corridor.
- TM7: **City to villages.** This move will improve the links to areas surrounding the city centre.
- TM8: **Harbour edge stitch.** This move provides a consistent vision for the continued development of Auckland's city centre waterfront.

The CCMP includes proposals to give effect to the transformational moves and outcomes. The following are relevant to appraising the proposal.

- (a) **Māori outcomes** includes **responding architecturally** to the natural and cultural context of Tāmaki Makaurau, including Māori **place names** and **te reo** (e.g. bi-lingual signage), **contributing to thematic approaches** to narrative and presence between the waterfront and Karangahape, promoting **Māori enterprise**, and applying **Te Aranga Design Principles**.
- (b) **Transforming Hobson and Nelson Streets into more liveable green twin boulevards.** It is intended to improve pedestrian amenity and connectivity as part of the east-west stitch.
- (c) **Widening the central city** from its traditional focus on a single main street by developing a parallel network including **Albert Street** as a complementary main business street, and a **laneway network** including Federal Street ridge laneway and the **east-west lanes** connecting the ridge with Queen Street (echoing the Tarapounamu path).
- (d) Promoting **development in proximity to public transport** (to maximise the benefits of investment in such transport). Areas promoted for such development includes those in proximity to the **Britomart Station** and **Ferry Terminal** and **'transit streets'** which includes Fanshawe Street.
- (e) Improving street and **laneway connections** between the central city and **public transport**.
- (f) **Public amenity improvements** and **connection** along **Quay Street**.

Auckland Plan 2050

The Auckland Plan is a long-term spatial plan to ensure Auckland grows in a way that will meet the opportunities and challenges of the future. While it is required by legislation, with a purpose of contributing to Auckland's social, economic, environmental and cultural well-being, the Auckland Plan is not a statutory planning provision and is an 'other matter'. It does not contain detailed actions (such as those in the City Centre Masterplan 2020) but it provides a high-level framework that can help interpret such details.

The Auckland Plan promotes the following six **outcomes**.

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- Outcome 1: **Belonging and Participation.** All Aucklanders will be part of and contribute to society, access opportunities, and have the chance to develop to their full potential.
- Outcome 2: **Māori identity and wellbeing:** Thriving Māori identity is Auckland's point of difference in the world – it advances prosperity for Māori and benefits all Aucklanders.
- Outcome 3: **Homes and places.** Aucklanders live in secure, healthy, and affordable homes, and have access to a range of inclusive public places.
- Outcome 4: **Transport and access.** Aucklanders will be able to get where they want to go more easily, safely and sustainably.
- Outcome 5: **Environment and cultural heritage.** Aucklanders preserve, protect and care for the natural environment as our shared cultural heritage, for its intrinsic value and for the benefit of present and future generations.
- Outcome 6: **Opportunity and prosperity.** Auckland is prosperous with many opportunities and delivers a better standard of living for everyone.

Directions and focus points are set out to give effect to each Outcome. Relevant directions for 'Homes and places' include:

- Direction 1: Develop a **quality compact urban form** to accommodate Auckland's growth
- Direction 2: **Accelerate the construction of homes** that meet Aucklanders' changing needs and preferences.

The Plan includes a 'Development Strategy' and 'Future Auckland' section that explains what is meant by '*a quality compact Auckland*', the benefits of such an approach, and how it is to be achieved:

- (a) '*Quality*' in this context means:
- most development occurs in areas that are **easily accessible by public transport, walking and cycling**
 - most development is within reasonable **walking distance of services and facilities** including **centres, community facilities, employment opportunities and open space**
 - future development maximises **efficient use of land**
 - delivery of **necessary infrastructure is coordinated** to support growth in the right place at the right time.
- (b) '*Compact*' means:
- future development will be focused within Auckland's urban footprint, with **most of that growth occurring in existing urban areas**

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- by 2050, most growth will have occurred within this urban footprint, limiting both expansion into the rural hinterland and rural land fragmentation.

(c) The **benefits** of a quality compact approach to growth and development are:

- greater **productivity and economic growth** – a compact urban form increases economic productivity from the greater proximity between firms, workers and consumers
- better **use of existing infrastructure** – growing within existing urban areas makes more efficient use of existing assets. Providing physical and social infrastructure costs less per household, which results in a higher overall level of service
- improved **transport outcomes** – a compact urban form brings more people closer to their place of work. Greater population density supports **faster, more frequent public transport** services. Both reduce congestion on the road network and create a **more efficient transport network overall**
- **rural productivity and character** can be maintained – encouraging growth within urban areas helps to protect rural environments from urban encroachment, and maintain the productive capability of the land and its rural character
- enhanced **environmental outcomes** – adverse effects of urban activities are concentrated into fewer receiving environments. Growth creates more opportunities for environmental enhancement, particularly as part of infrastructure upgrades
- greater **social and cultural vitality** – concentrating activity into urban centres and neighbourhoods provides a wider variety of activities to meet the full range of people's needs. This brings diversity and vibrancy into the urban environment which in turn enhances interaction and social cohesion.

(d) A 'quality compact Auckland' is to be achieved by:

- ensuring **sufficient capacity for growth** across Auckland
- embedding **good design** in all development
- **sequencing** what gets delivered
- aligning the **timing of infrastructure provision with development**
- supporting rural production.

(e) **Good design** is characterised as including the attributes of **functionality, attractiveness, longevity, innovation, and legibility**. It is to be **integrated at all scales** from city structure, to streets, to individual buildings. Design quality is integral to how the city functions which affects **overall wellbeing**. "*Good design can contribute to making Auckland a sustainable, attractive, equitable and desirable place.*" The Auckland Plan goes on to explain that "*the quality*

Isthmus.

*and characteristics of successful places make them **memorable**. They result in people going there more often, staying longer, or **choosing to live and work there**.”*

- (f) The Plan notes that, by 2050, growth will be particularly focused around identified locations including the **city centre**. It points to the **City Centre Masterplan** for a more specific vision for this area.

The Plan explains **Direction 2** (*accelerate the construction of homes that meet Aucklanders’ changing needs and preferences*) by noting that current construction falls well below demand. It says, “*we need to be **building more apartments**, including for individuals and large families, and townhouses, of **different sizes and at different price points**.”. It goes on to note that new construction will “*reflect the fact that Aucklanders’ **lifestyles and housing preferences are changing**. For example, there has been positive take-up of terraced housing and **apartments that are close to transport corridors and nodes**...”**

Te Aranga Principles

Te Aranga Māori Design Principles are practical design guidance based on Māori principles with the desire to enhance mana whenua presence, visibility, and participation in the design of the city. The principles include:

- Mana / Rangatiratanga - Authority; The status of iwi and hapū as mana whenua is **recognised and respected**
- Whakapapa - **Names & Naming**; mana whenua names are celebrated
- Tohu - The wider cultural landscape; Mana whenua **significant sites** and cultural landmarks are acknowledged
- Taiao - The **natural environment**; The natural environment is protected, restored and / or enhanced
- Mauri Tu - Environmental Health; Environmental health of water, land and air is protected, maintained and / or enhanced
- Mahi Toi - **Creative Expression**; Iwi/hapū narratives are captured and expressed creatively and appropriately in the physical environment
- Ahi Kā - **The Living Presence**; mana whenua are able to rekindle and strengthen their presence within the environment.

AUP Plan Change 78 matters are outlined and addressed within the AEE, prepared by *Barker & Associates Ltd.*

Appendix D

VISUAL STUDY (*refer separate document prepared by Warren and Mahoney Ltd*).